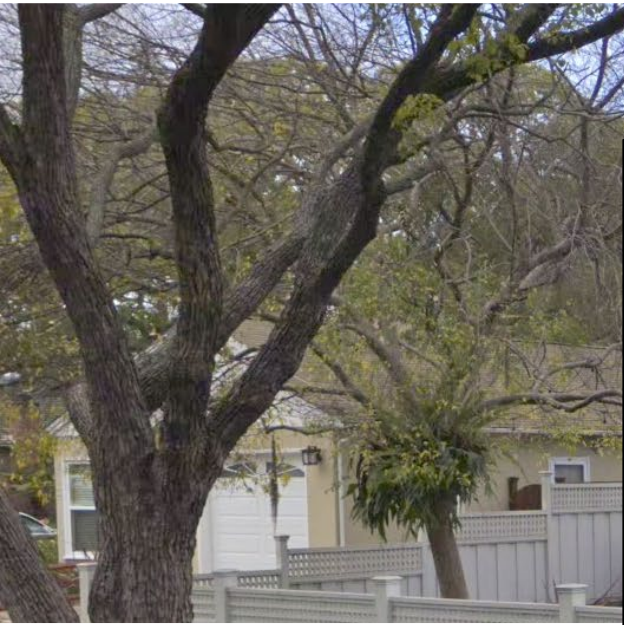


Finding the Missing Middle

October 17, 2018





- Lot size 5,009 sf
- 3 bdrm, 1 bath
- 1-car garage
- 960 sf
- Built 1950

-
- **\$1.4 Million**
 - \$1,458 per sf
 - Mortgage approx. \$6,400 per month
 - Rent \$4,000 per month





- In 1970 the median home price in California was 3 times a teachers salary, what is it today?

18 TIMES

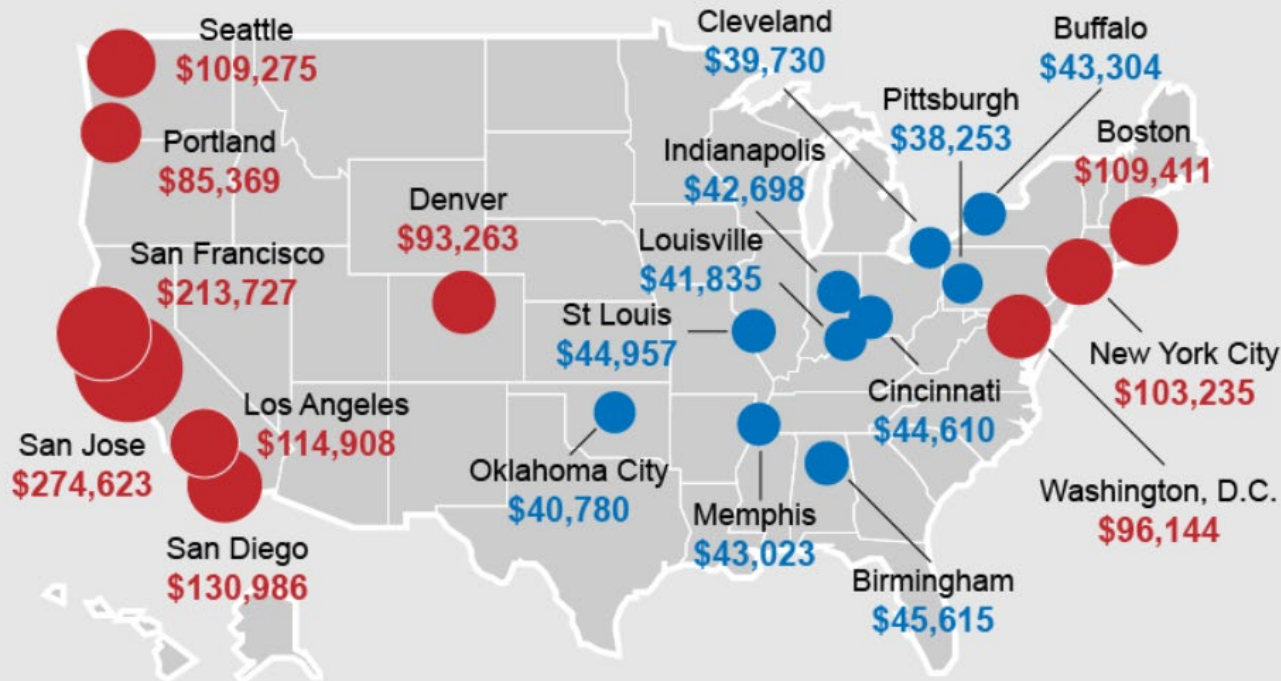
- What is the percent of families that can afford a median home price in San Mateo County?

< 13 %

Money Magazine: *The 15 most expensive places to buy a home in the US – July 2017*

1. San Jose, CA	1. \$1,070,000
2. San Francisco, CA	2. \$815,000
3. Anaheim, CA	3. \$750,000
4. Honolulu, HI	4. \$746,000
5. San Diego, CA	5. \$564,000
6. Boulder, CO	6. \$548,400
7. Los Angeles, CA	7. \$485,800
8. Long Island, NY	8. \$443,200
9. Naples, FL	9. \$435,000
10. Seattle, WA	10. \$422,100

The 10 **highest** and **lowest** required salaries to buy a median-priced house in the US.



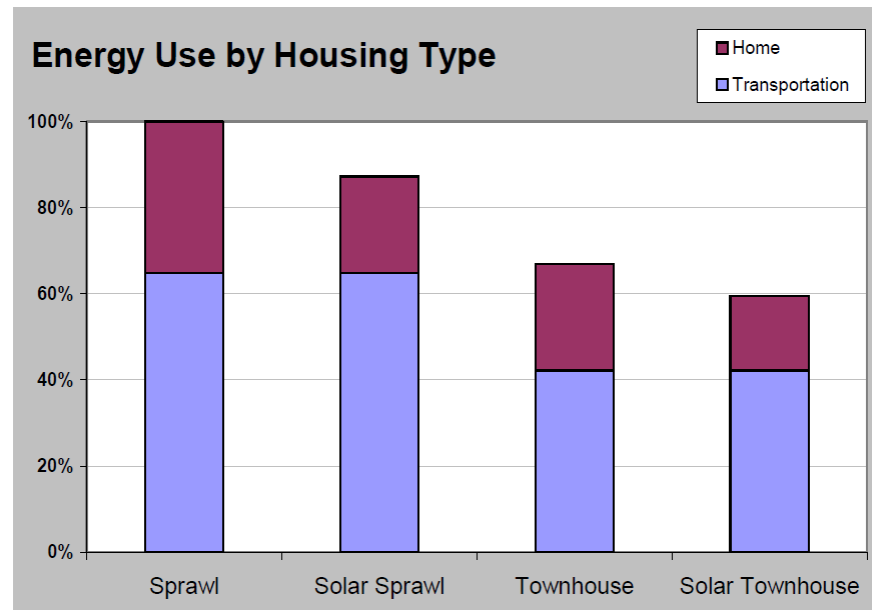
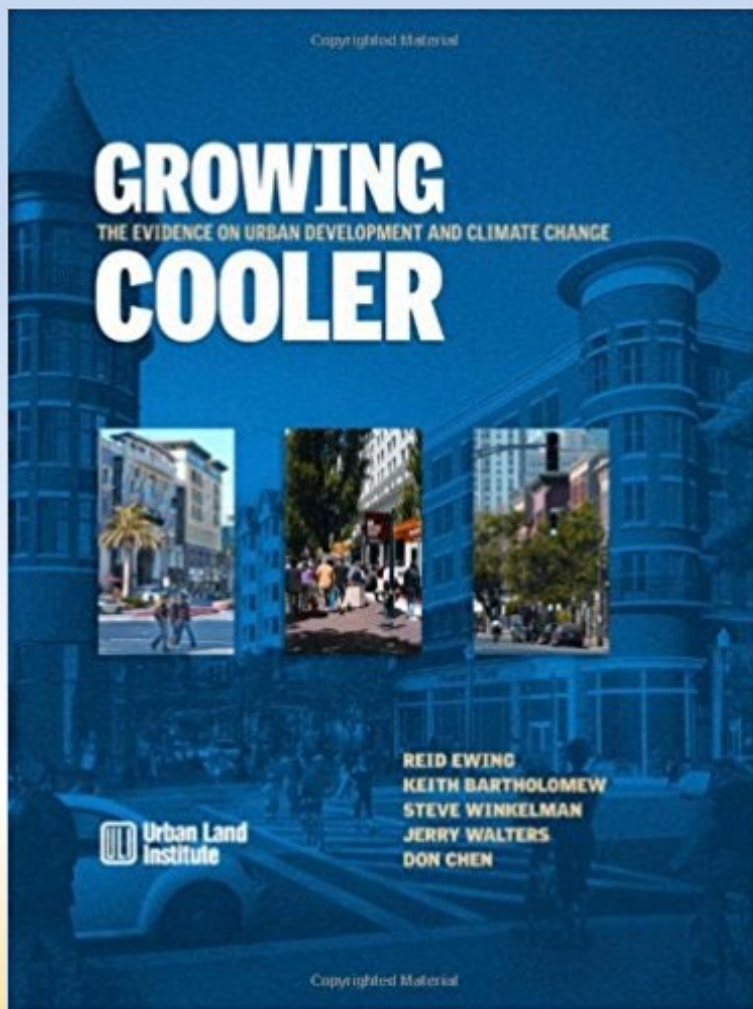
Urban Footprint — July 2018

- San Francisco Bay area has added 11 jobs for every home since 2010.
- Median home price in the SF Bay Area is \$934,000
- San Francisco \$1.4 Million
- Palo Alto \$3.1 Million
- Rents in SF have risen more than 60% since 2012.



United States - Trends

- Population Growth
 - Increase by 125M by 2050
- Built Environment
 - 50% of what is needed in 2050 does not exist today
- Energy Consumption
 - US consumes 25% of global oil demand
- Transportation
 - VMT increased 5x faster than population growth



Source: Peter Calthorpe

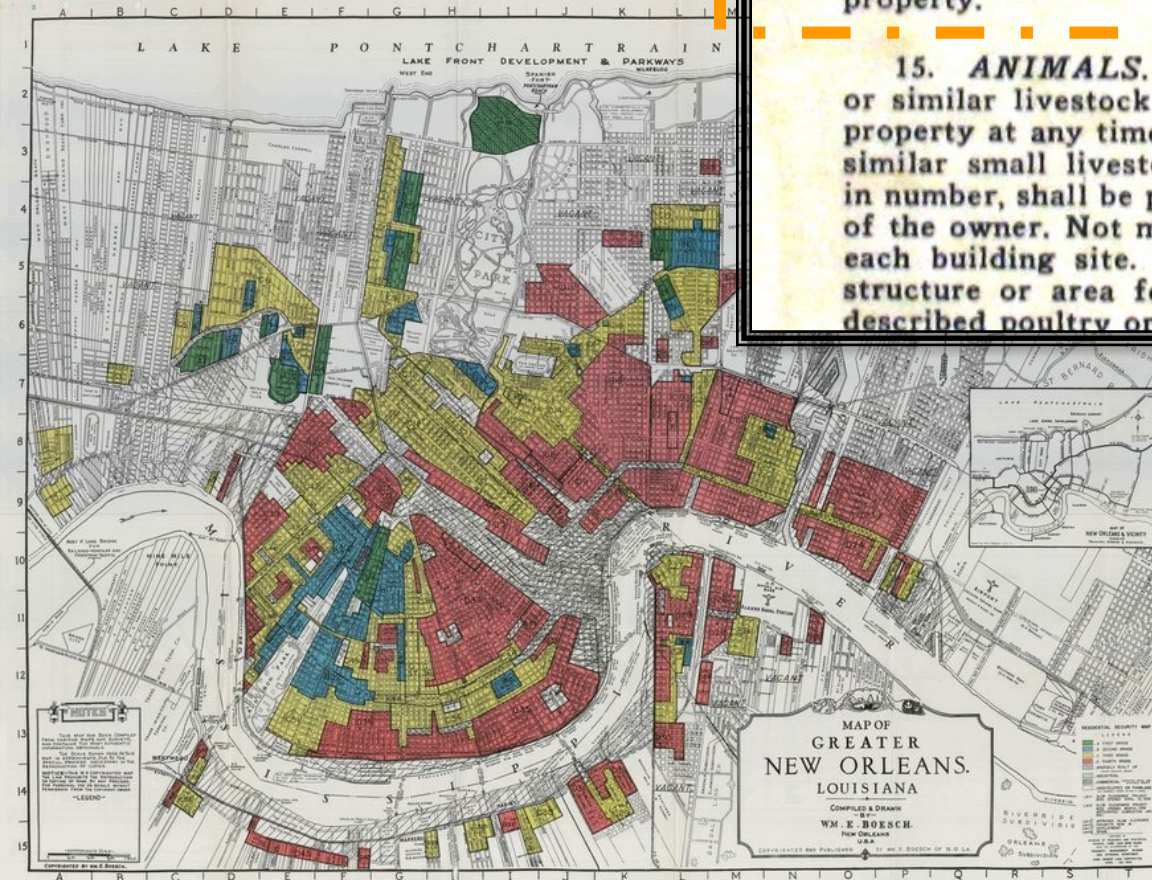
Historical Perspective


- Fair Housing Act
 - Redlining
 - Deed Restrictions
- White Flight
 - suburban sprawl
- Predatory Lending

said Tracts to a corporation or association formed by residents or owners of property in Innis Arden No. 2, or to a corporation or association formed by residents or owners of Innis Arden, for community purposes, in the activities of which corporation or association residents of Innis Arden No. 2 shall have the right to participate, subject to reasonable restrictions and requirements imposed by such corporation or association.

14. **RACIAL RESTRICTIONS.** No property in said addition shall at any time be sold, conveyed, rented or leased in whole or in part to any person or persons not of the White or Caucasian race. No person other than one of the White or Caucasian race shall be permitted to occupy any property in said addition or portion thereof or building thereon except a domestic servant actually employed by a person of the White or Caucasian race where the latter is an occupant of such property.

15. **ANIMALS.** No hogs, cattle, horses, sheep, goats, or or similar livestock shall be permitted or maintained on said property at any time. Chicken hens, pigeons, rabbits and other similar small livestock, not exceeding a total of twenty-five in number, shall be permitted but must be kept on the premises of the owner. Not more than one dog and cat may be kept for each building site. No pen, yard, run, hutch, coop or other structure or area for the housing and keeping of the above described poultry or animals shall be built or maintained closer



A photograph of a city street scene featuring two tall, modern apartment buildings with a grid-like facade. The street is covered in snow, and there are traffic lights and a few people visible in the distance. The text "Urban Renewal", "Projects", and "Public Housing" is overlaid on the image.

Urban Renewal

"Projects"

Public Housing



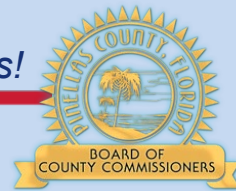
- Cabrini-Green Federal Housing Project
- Location: Chicago, Illinois
- Year Constructed: 1942-1962
- Number of Units: 3,500
- Site of Site: 70 acres
- Density: 50 units per acre
- Infamous for crime and squalid conditions
- Has been demolished (1995) and redeveloped because it was deemed unsalvageable



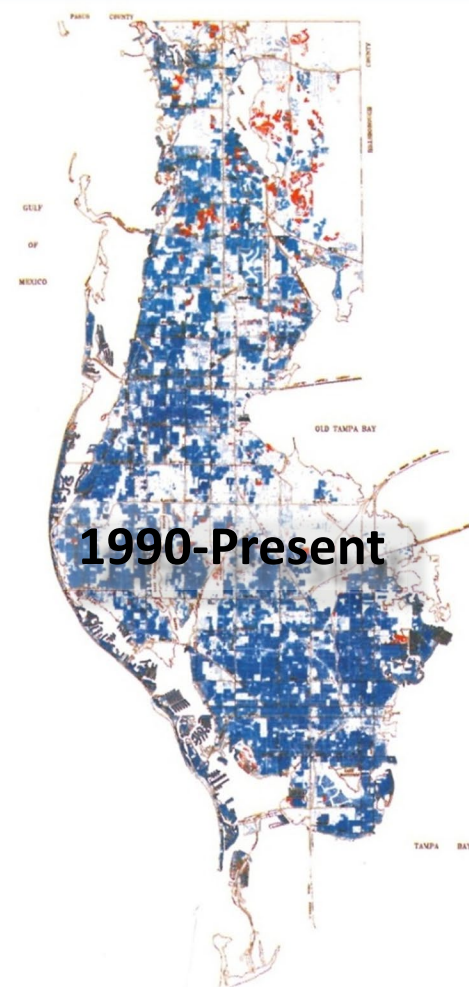
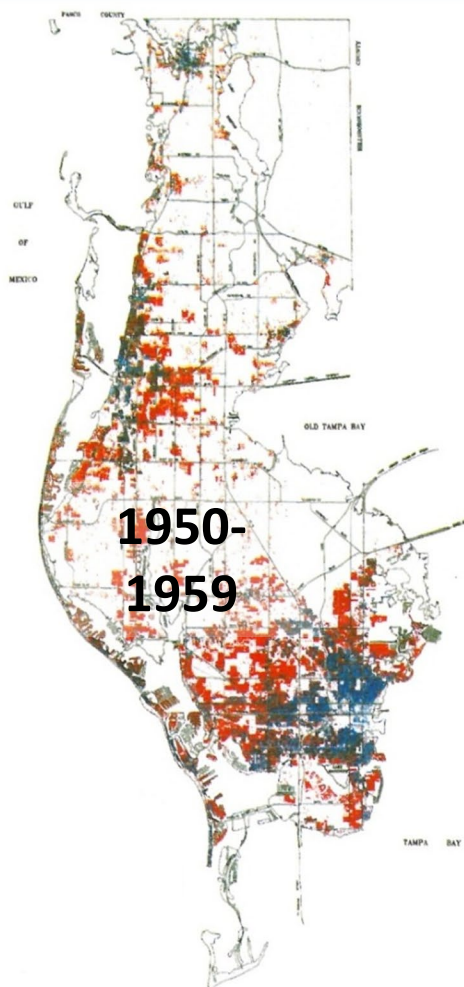
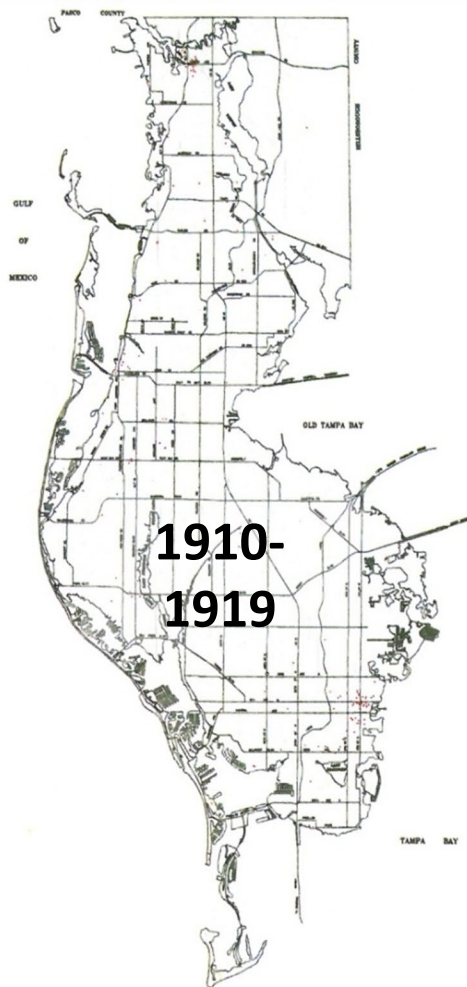
Our Vision: To Be the Standard for Public Service in America

Disposable Urbanism





Local Context



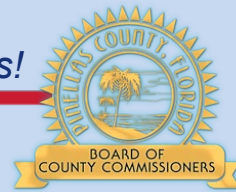
■ Existing residential units

■ New residential units









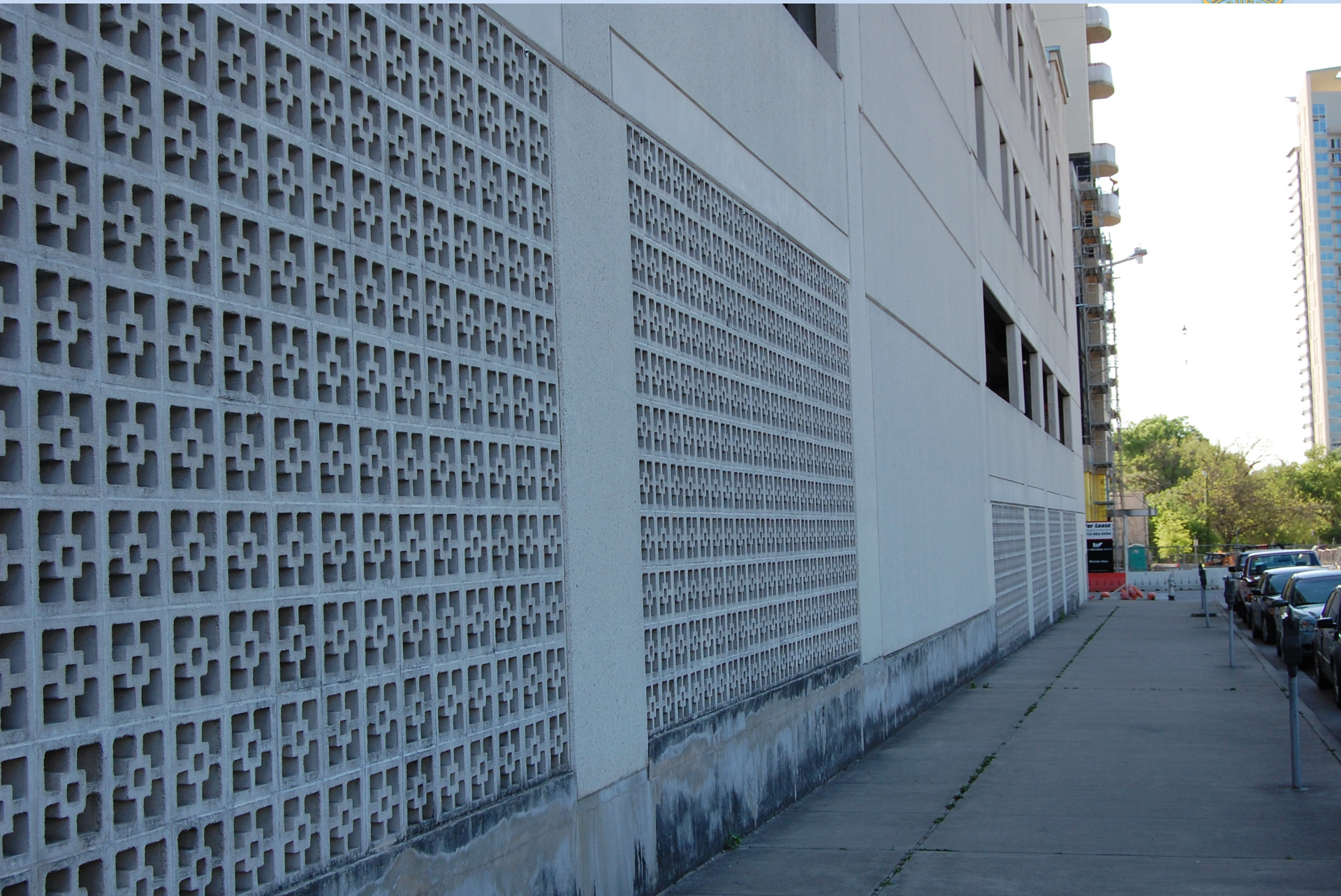
Florida's Growth Management

- Post WWII Boom
- Cost of Sprawl
 - Social
 - Health
 - Economic
 - Fiscal

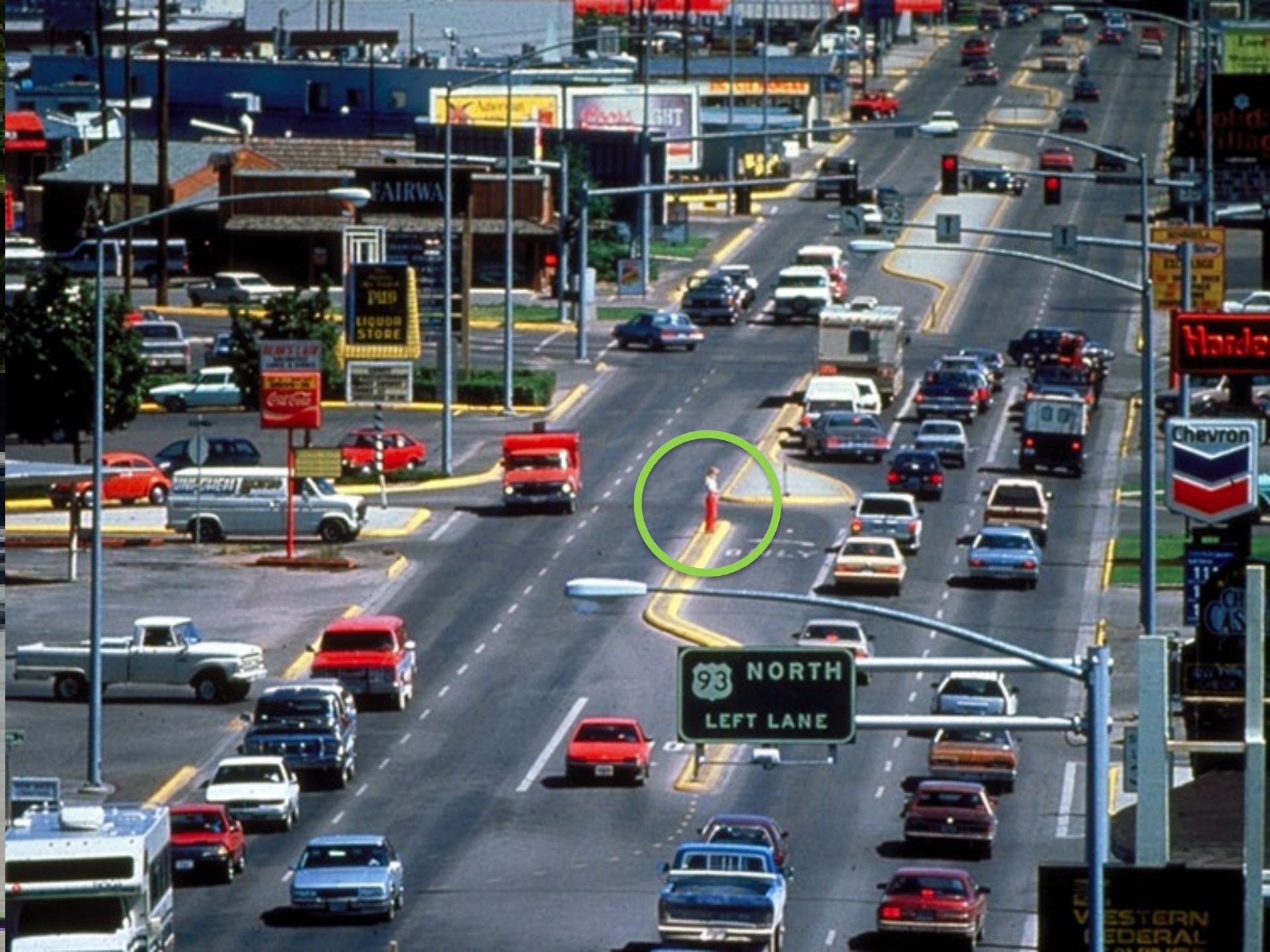


Vertical Sprawl









A photograph of a man riding a bicycle on a sidewalk. He is wearing a white t-shirt with a graphic, dark shorts, and a backpack. In the background, another person is riding a bicycle, and there are cars on the road. A large stop sign is visible on the right side of the frame. The scene is set in a suburban or urban area with a clear sky.

The Most Dangerous Place to Bicycle in America

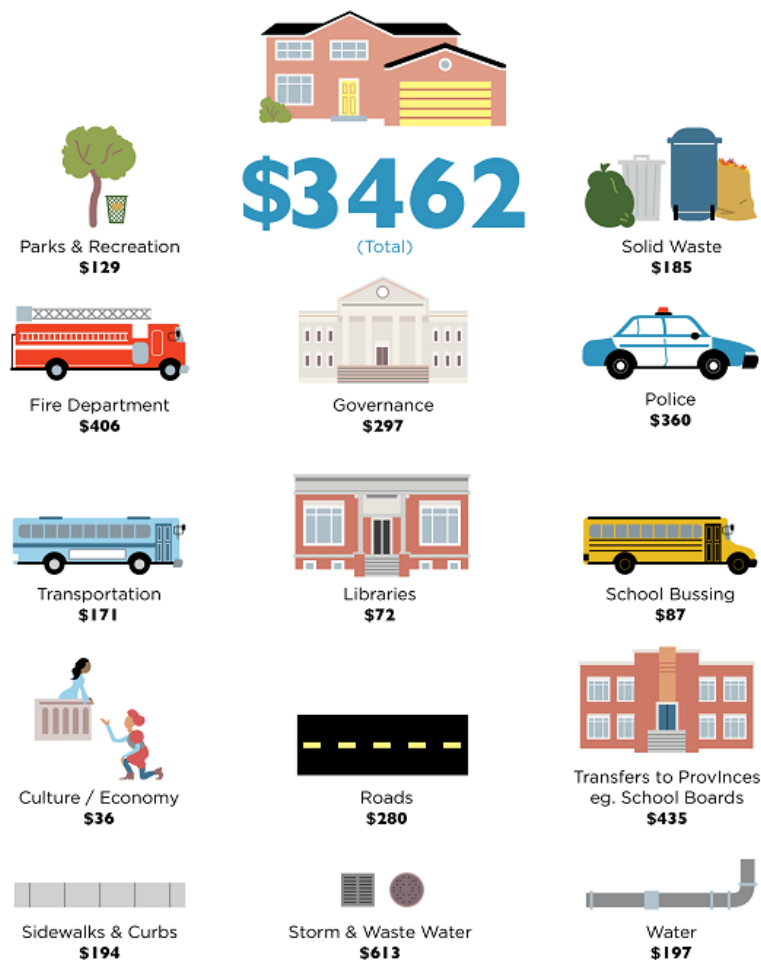
Pinellas County, Fla., has the highest cyclist death rate in the Tampa Bay metro area—which has the highest rate of any metro region in the U.S.



Our Vision: To Be the Standard for Public Service in America

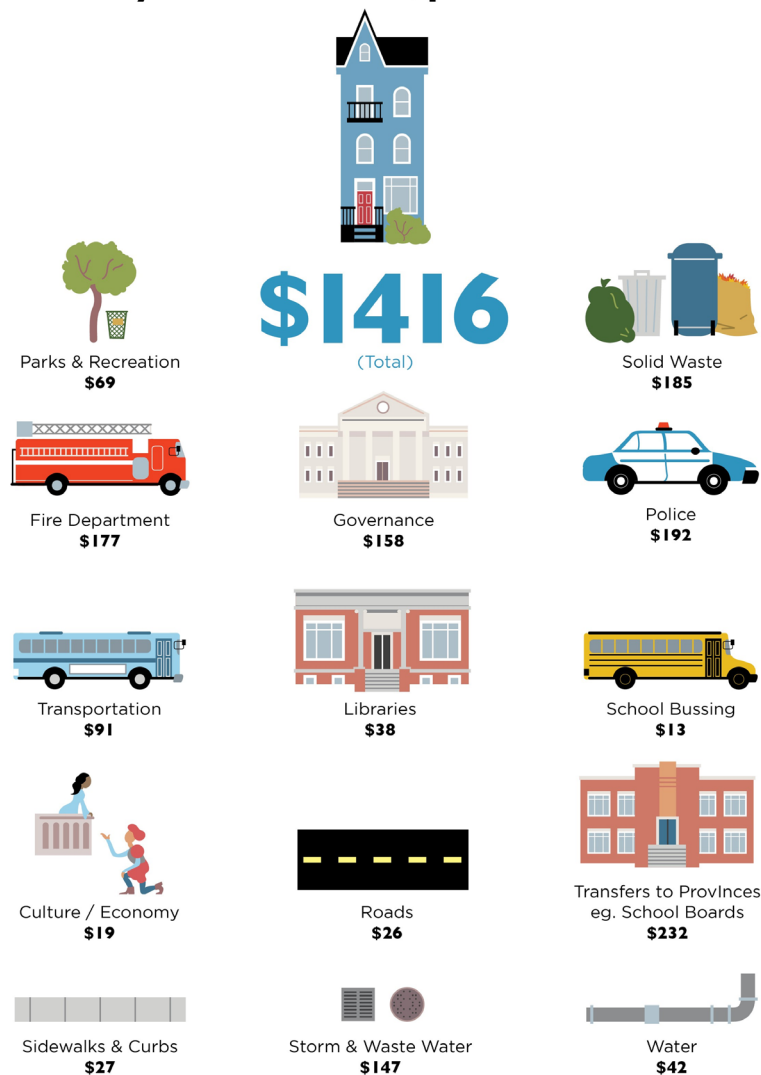
Suburban

City's Annual Cost, per Household



Urban

City's Annual Cost, per Household



Redefining Density

Density



Scale and Intensity



THE WORLD'S POPULATION, CONCENTRATED

If the world's 6.9 billion people lived in one city, how large would that city be if it were as dense as...





Simplicity + Variety = Authenticity



Simplicity - Variety = Monotony



Livable Density

- Design
 - Context sensitive urban design
 - Streetscape
 - Linking transportation networks
- Social Infrastructure
 - Libraries, Schools, Sports Venues, etc.
 - Access to opportunities
- Creating Economic Value
 - Value Capture

“MISSING MIDDLE”



www.missingmiddlehousing.com

www.missingmiddlehousing.com



www.missingmiddlehousing.com



www.missingmiddlehousing.com



www.missingmiddlehousing.com



www.missingmiddlehousing.com



Town House

www.missingmiddlehousing.com

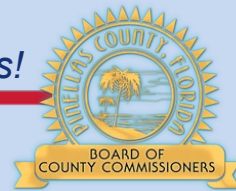


Live / Work Units

www.missingmiddlehousing.com



Courtyard Apartments



Illustrative Examples



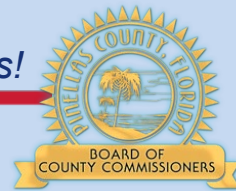








Doing Things!



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Dunedin



Dunedin – The Artisan



Dunedin - Glencairn



Oldsmar – Hayes Park Village



Safety Harbor

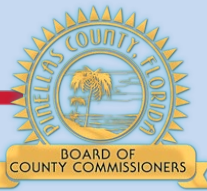


**Scheme A - Concept Design for
Luxury Apartment Building
Main and 2nd Ave
Safety Harbor, FL**



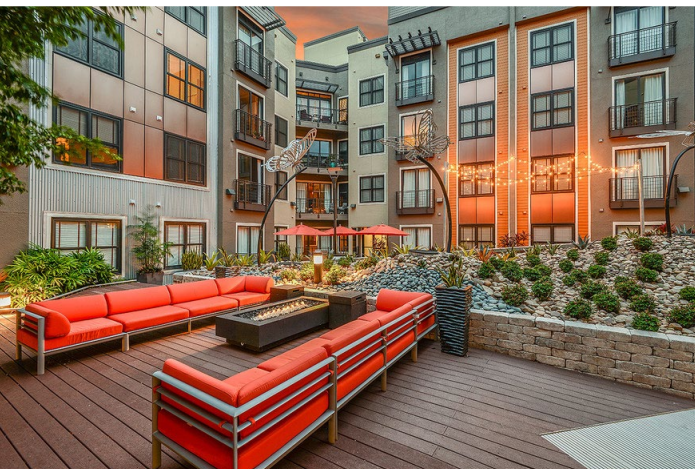
Largo

Doing Things!

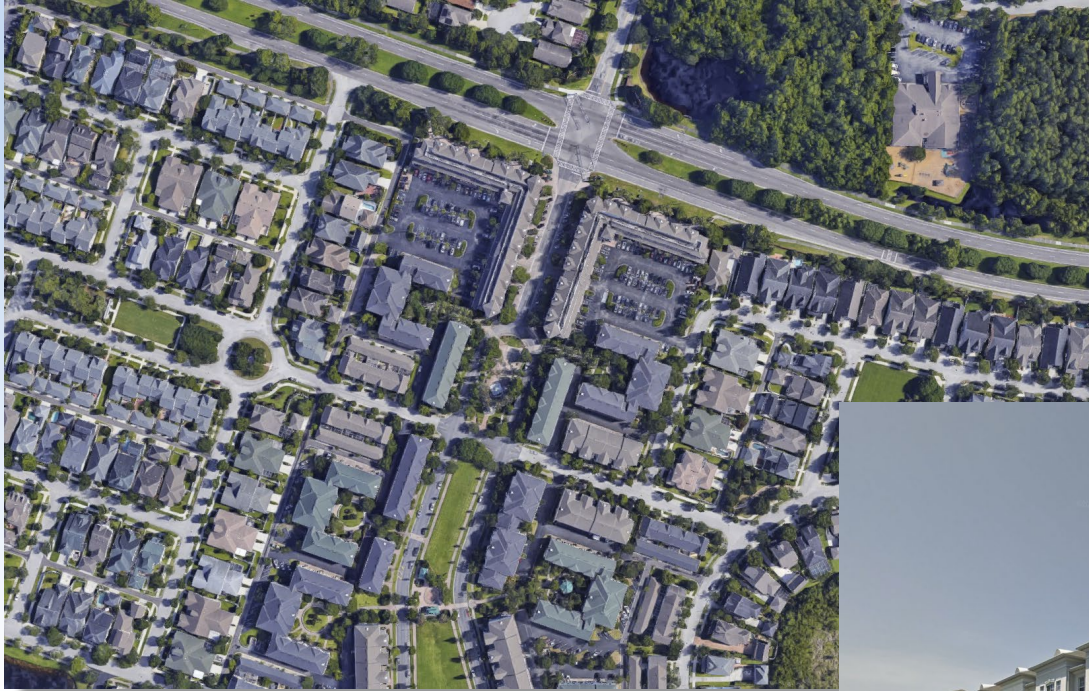


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St. Petersburg



Westchase



Coding for the Missing Middle

Conventional vs. Form Based Codes

Sec. 138-1281 - Measurement of setbacks.

- Setbacks shall be measured by the shortest dimension, running from the property line to the structure.
- No portion of an alley shall be considered as part of a required setback.
- For determination of setbacks, corner lots and multiple-frontage lots shall be considered to have fronts on all street frontages unless otherwise specified in this chapter. Side setbacks shall apply to all other sides of such a lot or parcel.
- Where right-of-way lines are established by action of the board of county commissioners for the purpose of future roads or widening of existing roads, all street setbacks shall be measured from the proposed right-of-way line.
- Sills, eaves, cornices, chimneys, flues, mechanical equipment and similar projections may project into a setback area not more than three feet and shall not extend over adjacent property.
- Wing walls shall conform to the normal setback requirements whenever they exceed the allowable height of a fence (see section 138-1336).
- An open, unroofed porch, patio, or paved terrace may project into a required front setback for a distance not exceeding ten feet.
- All residential structures, and their accessory structures, on waterfront lots or parcels shall be set back 25 feet from the mean high water mark in tidal areas or normal high water on lakes; except where adequate seawalls or riprap stabilization exist, the setback requirement shall be 15 feet from the seawall or stabilization. Pools may be constructed pursuant to subsection (i) of this section provided that certification from an engineer registered in the state, stating that the proposed structure will not affect the integrity or functioning of the seawall or its deadmen, is submitted prior to issuance of a permit.
- Requirements for residential accessory uses. Only one utility shed or storage building shall be permitted accessory to a residence and shall meet the requirements set forth in the Pinellas County Code, section 22-202. Utility sheds of 100 square feet or less and no higher than ten feet may be located with no setback from a side or rear property line which is enclosed with an opaque six-foot high fence or wall. Where no such fence exists, utility sheds of 100 square feet or less and no higher than ten feet shall be setback at least two feet from the property line. All such sheds shall be designed in a manner so that water runoff from the roof of the structure is not directed onto neighboring properties. All other sheds and accessory residential structures shall meet setbacks as required by the zoning district where located, or as otherwise provided in this chapter except that utility sheds of 100 square feet or less and no higher than ten feet that have been in place for at least seven years shall be considered a legitimate nonconforming use subject to the standards set forth in this chapter governing such nonconformities. Screen-only enclosures may be located within a required side or rear yard provided a minimum setback of five feet is maintained from the side or rear property line. Pools may be located within a required rear yard provided a minimum setback of eight feet is maintained from the rear property line. When located on lots with frontage on two streets on the opposite sides of the lot, pools and screen-only enclosures may be located within 15 feet of the public right-of-way in the area of the lot which is commonly considered the rear yard.
- Wherever a side or rear lot line in a commercial or industrial district abuts a railroad right-of-way, a railroad siding track, or a railroad easement, the side and rear setback requirements will not apply at the abutting side or rear line and construction of buildings will be permitted up to the abutting side or rear property line.
- No portion of any structure shall be located within the area of a recorded public easement unless authorized by the county engineering department, the department of environmental management or other appropriate agency. This requirement shall not be varied by the board of adjustment.
- On a parcel which does not abut a public right-of-way, the required front setback shall be measured from the edge of the roadway or easement edge, whichever is greater, except as otherwise provided in this chapter.
- For nonconforming setbacks, see section 138-208.

Sec. 138-351 - Residential infill development.

This section may be applied to all residential districts as a goal to create compatible and harmonious infill development and redevelopment in established residential neighborhoods. The setback and height requirements of residential districts may be administratively adjusted to allow development and redevelopment to occur in concert with abutting properties. In cases where properties exist in established residential neighborhoods, development of said properties may occur to be compatible with abutting lots in terms of setbacks and height adjustments.

- The proposed infill development may conform to any standards required by valid recorded plans, deed restriction or approved valid site plans, to the extent provided by law or;
- Where such documentation is not available, the setbacks of the proposed infill units shall be based upon the average setbacks of abutting units. [Example: if a proposed infill lot abuts two single-family homes with front setbacks of ten feet and 20 feet, the proposed unit may be constructed with a minimum 15 feet front setback.] This standard shall be applied to the primary structure; accessory structures may not be used in determining the average setback.
- When a primary structure is constructed using a reduced setback afforded by this section, the structural height shall be limited by the average stories/levels of the primary structures on the abutting properties, rounded to the highest story/level. In this case, a structure that utilizes the reduced setback shall be limited to such average stories/levels and may not necessarily be permitted the full building height of the district. In no case shall building height exceed the maximum for the district. This standard is intended to achieve compatible infill development.
- The property owner may pursue the development flexibility afforded in this section by providing proper documentation to the applicable county reviewing department. Proper documentation may include official surveys, development plans, blueprints or other documentation as may be approved by the county administrator or designee.

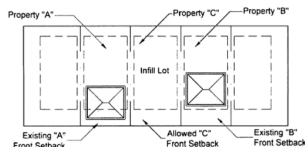
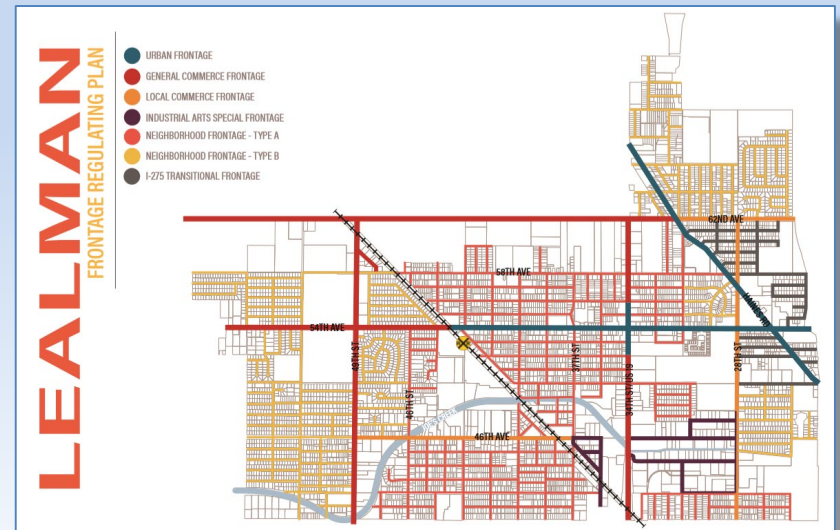


Figure 138-351(d)1—Residential Infill Standards

Property "C" (infill lot) may be permitted a reduced front structural setback based on the average existing front setbacks of adjacent properties "A" and "B". The average setback of the adjacent properties shall be calculated as follows:

$$(\text{Existing Front Setback "A"} + \text{Existing Front Setback "B"}) / 2 = \text{Allowed Front Setback "C"}$$

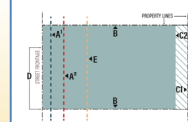


URBAN FRONTAGE

INTENT

The "Urban Frontage" applies to two corridors within Lealman, 54th Avenue N, and Haines Road. This type is also applied to the 54th Avenue N / 34th Street N intersection to create a dense, urban node. This frontage type is intended to support redevelopment and enhance the corridors as dynamic pedestrian friendly environments that provide entertainment, goods, and services community wide. Properties along this frontage type will typically be built with minimal to no front setbacks, parking to the rear or side of buildings, and go 3 stories in height, to allow for mix of uses and increased residential density. The 54th Avenue corridor has the potential to serve as Lealman's "Main Street" due to its direct access to I-275 and surrounding communities. This street of 54th Avenue can support a mix of uses. Bikes that turn at an angle, north to south, making this street unique in character. This street is also in close proximity to I-275 which lends it to be more urban in character.

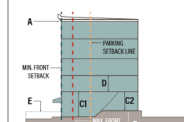
SITING



REQUIREMENT	MIN.	MAX.
A Front Setback		
A 54th Avenue N / 34th St. N, Haines Road (34th St. N to 35th St. N)	5' 0"	10' 0"
B Side Setback	-	-
C Rear Setback (Any)	-	-
C2 Rear Setback (No Alley)	1' 0"	-
C3 Rear Setback (No Alley)	1' 0"	-
D Building Facade*	75%	-
E Parking Setback	20' 0"	20' 0"
F Height	20' 0"	20' 0"

- A pedestrian treatment shall be required within the 5.4 mile urban front setback along 54th Avenue N and 34th Street N.
- If a building has an existing front facade may be reduced to a minimum of 5 feet from the build to line, along Haines Road.
- Landscaping or other treatment shall be required along the existing 54th Avenue N and 34th Street N frontages and required to include this along side, as per the 2020 Street Code, shall be provided for the property of the subject property is permitted direct access from 54th Avenue N.

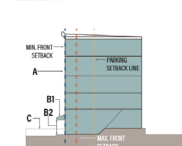
HEIGHT



REQUIREMENT	MIN.	MAX.
A Building Height	1 story	5 stories
B Ground Floor Facade (Residential Use)	2' 0" above BFE	-
C1 Ground Floor Facade* (Non-Residential Use)	2' 0" above street height	-
C2 Ground Floor Facade (Residential Use)	2' 0" above street height	-
D Upper Story Height	-	-
E Front Yard Fence/Wall Height	-	4' 0"

- The story structure shall have a minimum of 10' front setbacks.

ELEMENTS



REQUIREMENT	MIN.	MAX.
A Frontage		
A Frontage		
B Building Projections		
C Front Yard Fence/Wall		

USE CATEGORIES*

- Ground Story Uses:
- Neighborhood Commercial
 - General Commercial
 - Industrial
 - Office
 - Residential
- Upper Story Uses:
- Residential
 - Neighborhood Commercial
 - General Commercial
- * Refer to the Urban Frontage Code for full list of permitted uses within each use category.
- * Refer to the Urban Frontage Code for full list of permitted uses within each use category.

ADDITIONAL STANDARDS

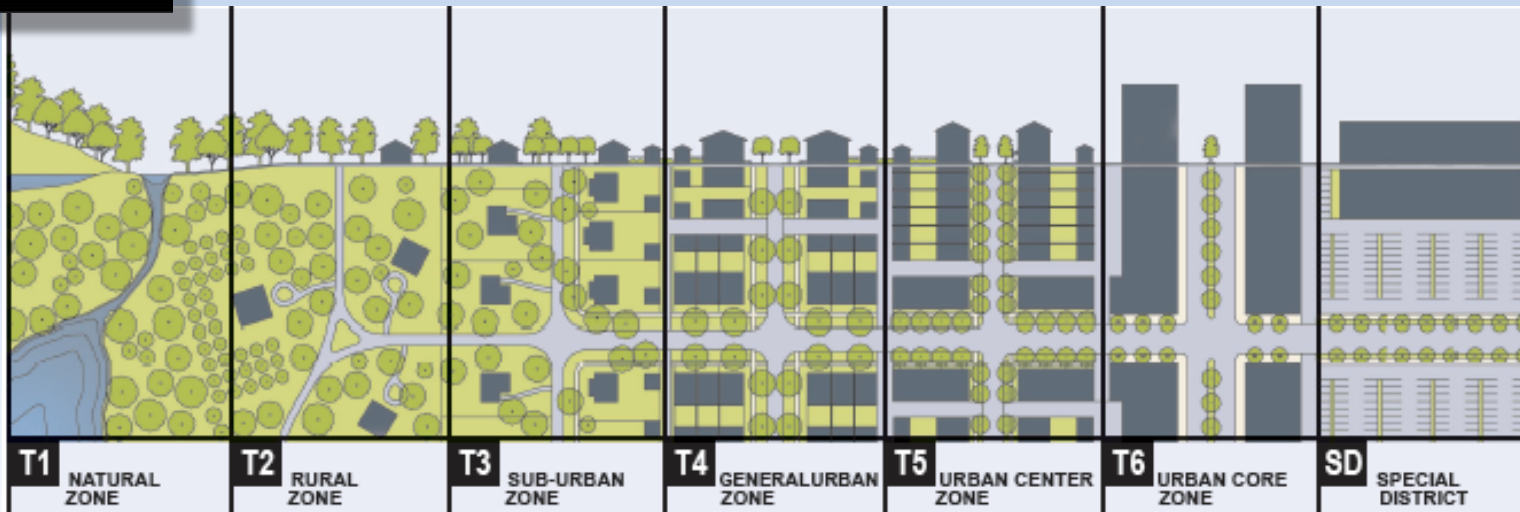
- Vehicle Access
 - Vehicle access is prohibited from 54th Avenue N, unless a property does not have any other means of access. Vehicular access shall be obtained from either an alley (if applicable) or from the street frontage that has the lowest thoroughfare classification. In the event that the existing driveway is unimproved, the alley shall be improved up to the access point of the property as part of the development.
 - If a property currently gains vehicle access from a shared driveway, access shall continue to be shared or further consolidated in the event of redevelopment of the subject property.
- Building Orientation
 - All front facades shall be oriented parallel to the Haines Road street.
- Pedestrian Access
 - A minimum 5 foot wide pedestrian connection to the public sidewalk shall be provided.
- Fence/Coverage Location
 - All fences, associated with a front facade, shall be located behind the front facade structure.

Livable Density

- Design
 - Context sensitive urban design
 - Streetscape
 - Linking transportation networks
- Social Infrastructure
 - Libraries, Schools, Sports Venues, etc.
 - Access to opportunities
- Creating Economic Value
 - Value Capture

SMARTCODE
VERSION 9 AND MANUAL

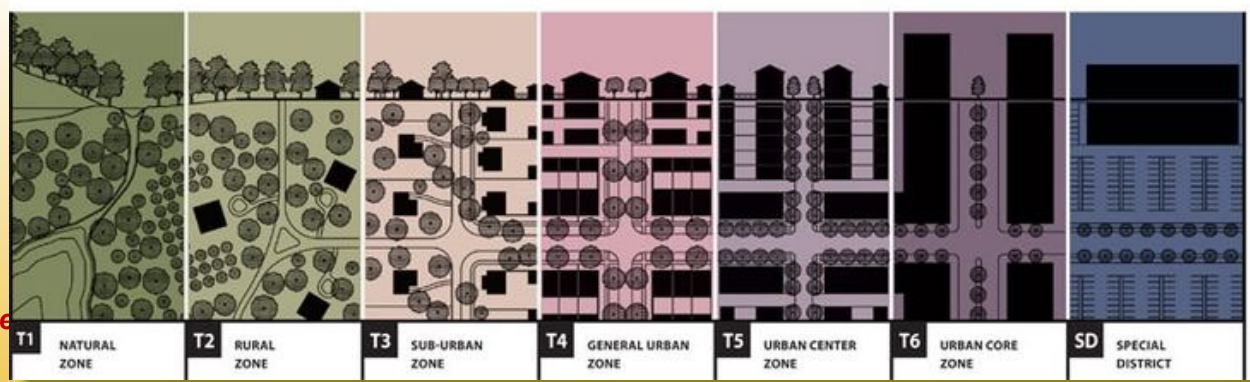
SmartCode



A TYPICAL RURAL-URBAN TRANSECT, WITH TRANSECT ZONES



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Additional Resources

Consultants

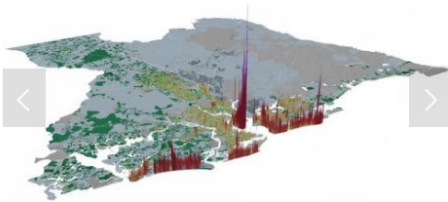
Urban3

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CONTACT

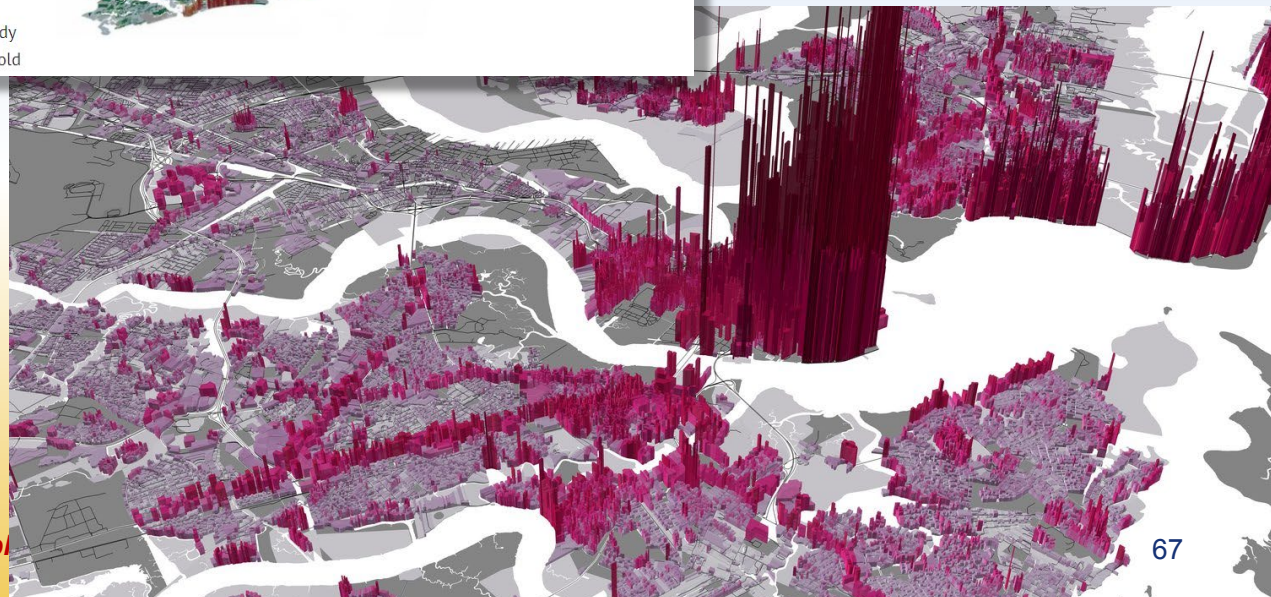


Charleston, SC

Charleston has a long and rich history. It is one of the United States' oldest cities and has excelled as a port city, a trade city, an industrial city and now a tourism city. Charleston has continuously evolved to maintain its authenticity, and protect its place as one of The South's premier cultural homes. The City has survived fires, wars, hurricanes, and even a devastating earthquake, only to come back stronger. Charleston is an interesting case study because of its historic pattern of development. Being an old




<http://www.urban-three.com>



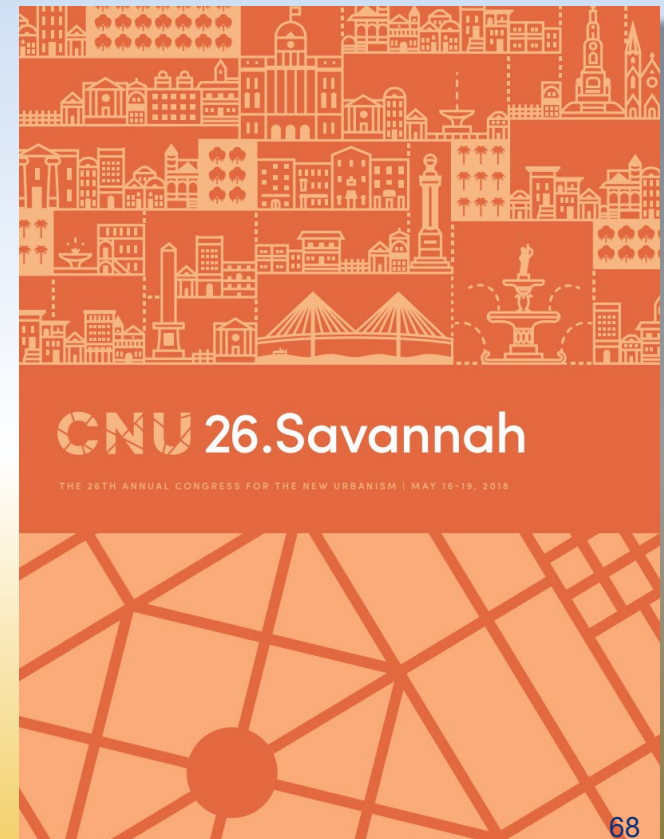
Webinars, Conferences

SPRAWL REPAIR FOR FLORIDA'S COMMUNITIES

DESIGN, REGULATORY AND IMPLEMENTATION
TOOLS

1000 FRIENDS OF FLORIDA
NOVEMBER 18, 2015

GALINA TACHIEVA
DPZ PARTNERS



Organizations

- Congress for the New Urbanism www.cnu.org
- Urban Land Institute www.uli.org
- Form Based Code Institute www.formbasedcodes.org
- American Planning Association www.planning.org
- Planetizen www.planetizen.com
- Rail-Volution www.railvolution.org
- Strong Towns www.strongtowns.org
- Smart Growth America www.smartgrowthamerica.org

Publications

ELLEN DUNHAM-JONES AND JUNE WILLIAMSON

RETROFITTING SUBURBIA

URBAN DESIGN SOLUTIONS *for* REDESIGNING SUBURBIA

UPDATED
EDITION

WITH A NEW UPDATE BY THE AUTHORS AND A FOREWORD BY RICHARD FLORIO

Designing
Suburban
Futures
New Models
From Build a
Better Burb
June Williamson

EDITED BY EMILY TAKEN

Retrofitting Sprawl

Addressing
Seventy Years of
Folded Urban Form

Podcasts



Blake Lyon

Director of Development Review Services

blyon@pinellascounty.org

727-464-6053