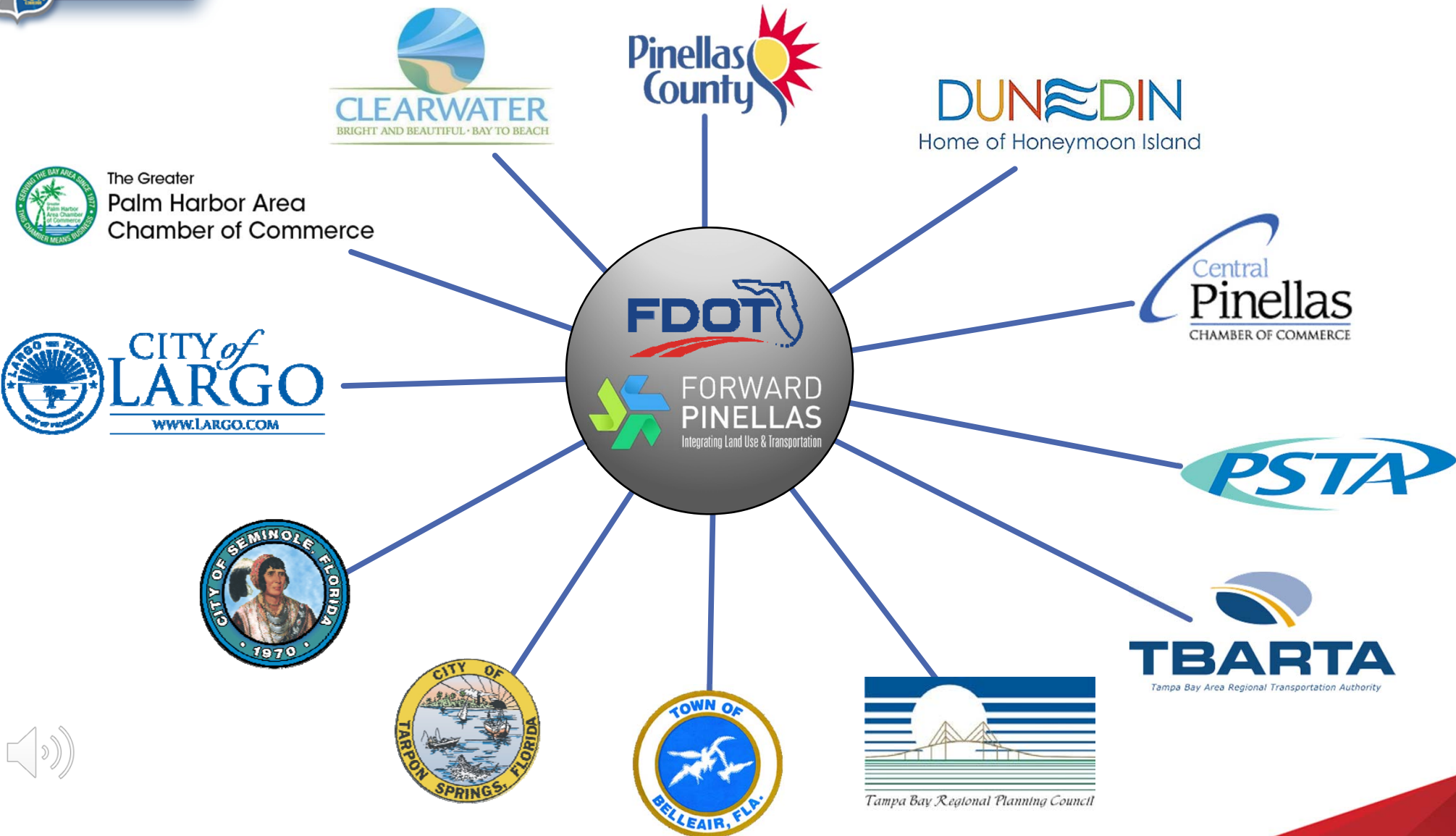


# Alternate US 19 from Park Street N. to Pinellas/Pasco Co. Line Pinellas County, FL



Forward Pinellas  
Board Meeting  
February 13, 2019







# Presentation Outline

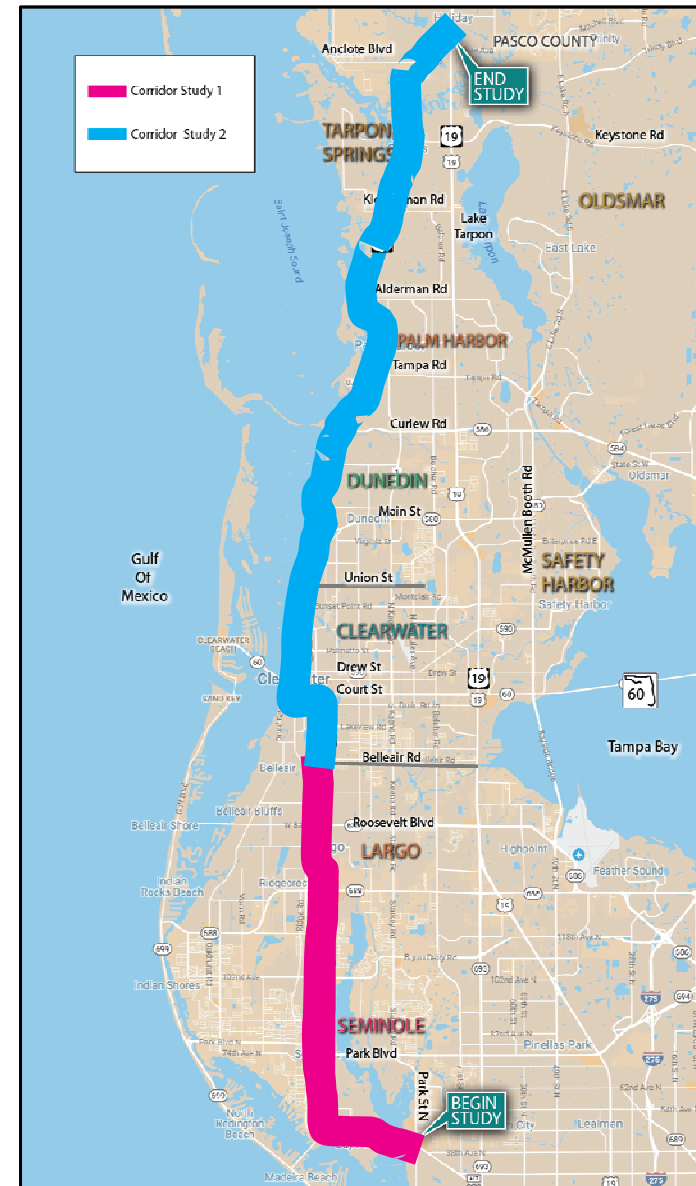
- ▶ Project Overview
- ▶ Public Outreach and Data Collection
- ▶ Corridor Vision
- ▶ Proposed Concepts
- ▶ Next Steps





# Alternate US 19 Corridor Studies

- ▶ Corridor Study I Limits: from Park Street North to Belleair Road - 11 miles
- ▶ Corridor Study II Limits: from Belleair Road to Pinellas/Pasco County Line - 17.9 miles







# Preliminary Purpose and Need

## ► Project purpose:

- (1) Address near-term multimodal transportation needs through context sensitive solutions; and
- (2) Develop a long-term corridor vision that defines the goals & objectives and policy for enhanced integration of land use and transportation.



Stakeholder  
Outreach

Goals & Objectives / Policy

Visioning  
Charrettes

Vision Workshop

Vision Plan

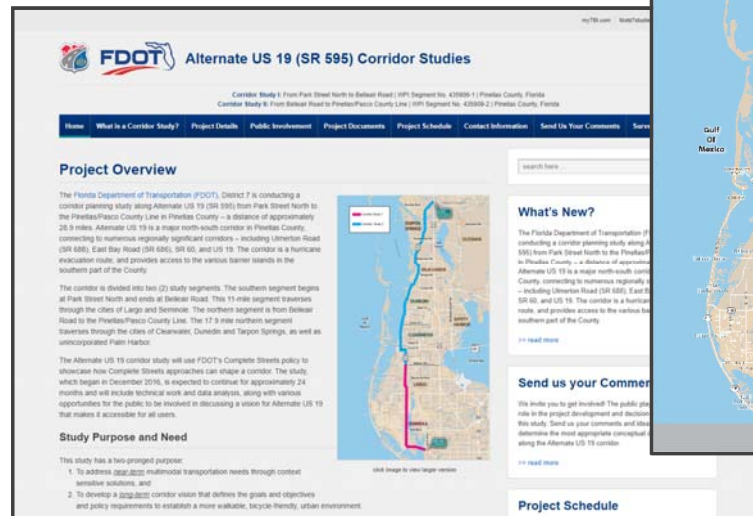
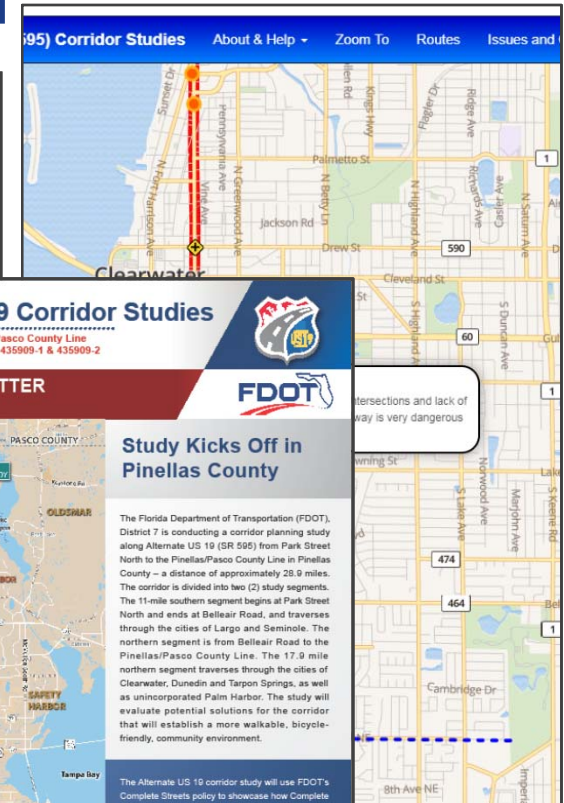
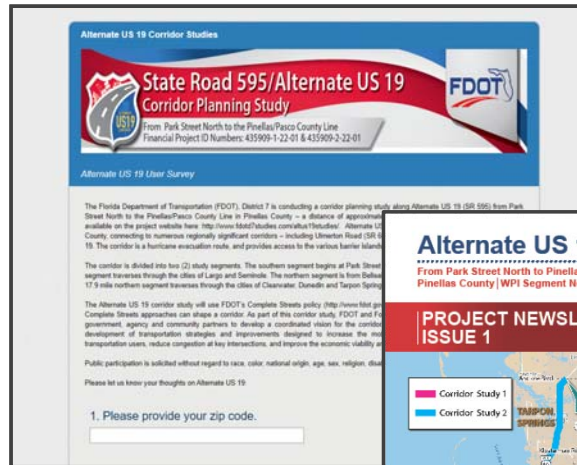
Alternatives  
Strategies

Corridor Studies  
Alternatives  
Workshop



# Public and Stakeholder Outreach

- ▶ Project Website
- ▶ Online User Survey
- ▶ Visioning Charrettes
- ▶ PAG Meetings
- ▶ WikiMapping Tool
- ▶ Stakeholder/Public Meetings
- ▶ Newsletters
- ▶ Postcards





# Comments Received to Date

► What **hot topics** have we been hearing?

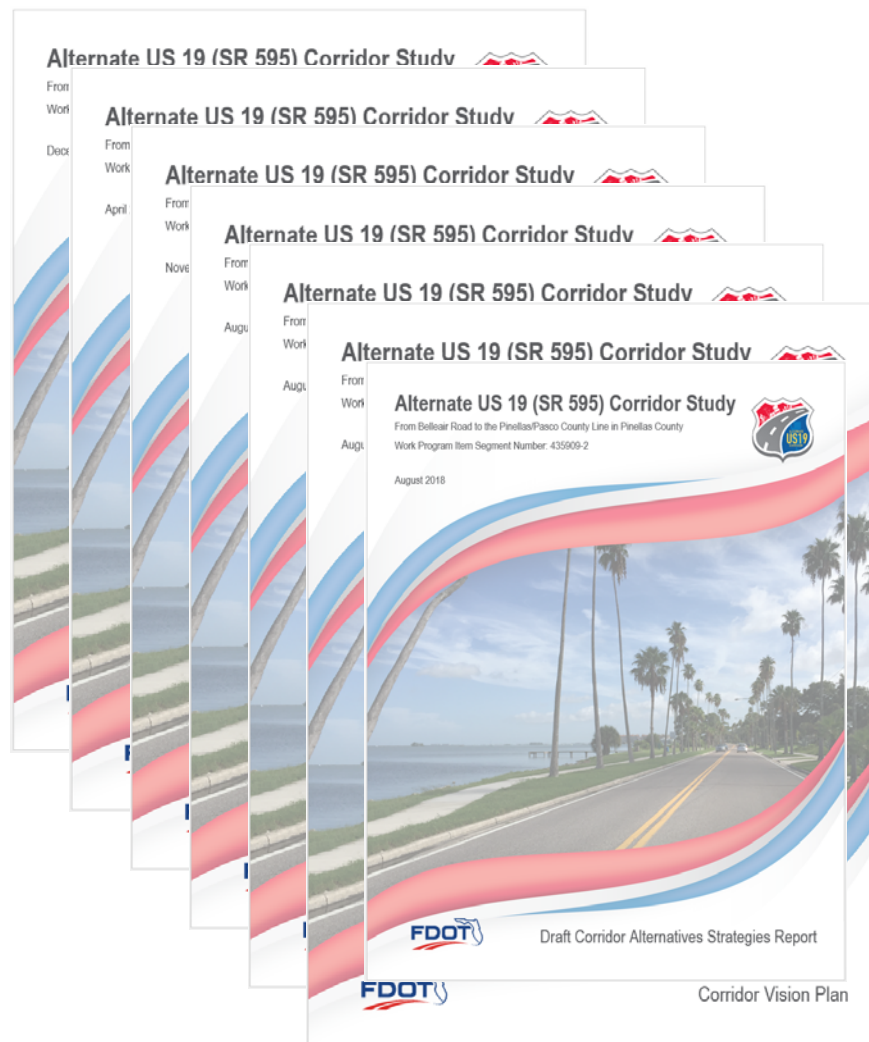
	Bike/Ped Safety & Amenities	Mobility Issues/ Congestion	Connectivity	Balance User Needs	Roundabouts	Lighting
Largo	★	★				
Seminole	★	★				
Clearwater	★		★			
Dunedin	★		★	★		
Unincorporated Palm Harbor	★				★	★
Tarpon Springs	★				★	★
Corridor Wide	★	★	★	★	★	

Others: Trail safety and amenities; prioritizing local traffic (over truck traffic); alternate north-south corridors as relievers



# Technical Analyses






- ▶ Existing Conditions Analysis
- ▶ Traffic Analysis
- ▶ Safety Analysis
- ▶ Drainage Analysis
- ▶ Environmental Analysis
- ▶ Nodal Planning Analysis
- ▶ Corridor Alternative Strategies





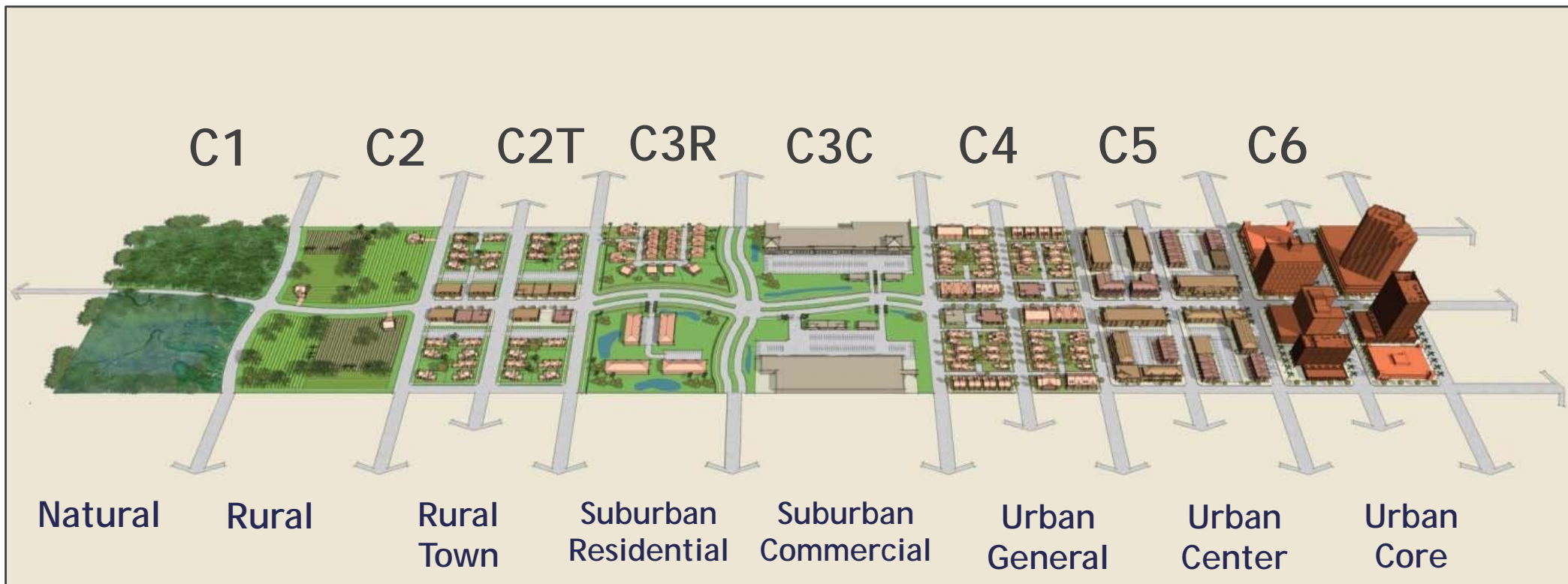


# The Vision Plan - Guiding Principles

	Match the Character	Match future street improvements to the activities and character of the surrounding context.
	Increase Safety	Use street improvements to increase the level of safety for all users of the Corridor.
	Balance User Needs	Create multiple typical sections that balance the needs of all users within the Corridor.
	Enhance the Networks	Use existing and future development to enhance the transportation networks and promote connectivity throughout the Corridor.
	Celebrate the Unique Assets	Protect and integrate historic, cultural, and environmental elements within the Corridor.



# What are the FDOT Context Classifications?



Alternate US 19 from Park Street N to Pinellas/Pasco County Line

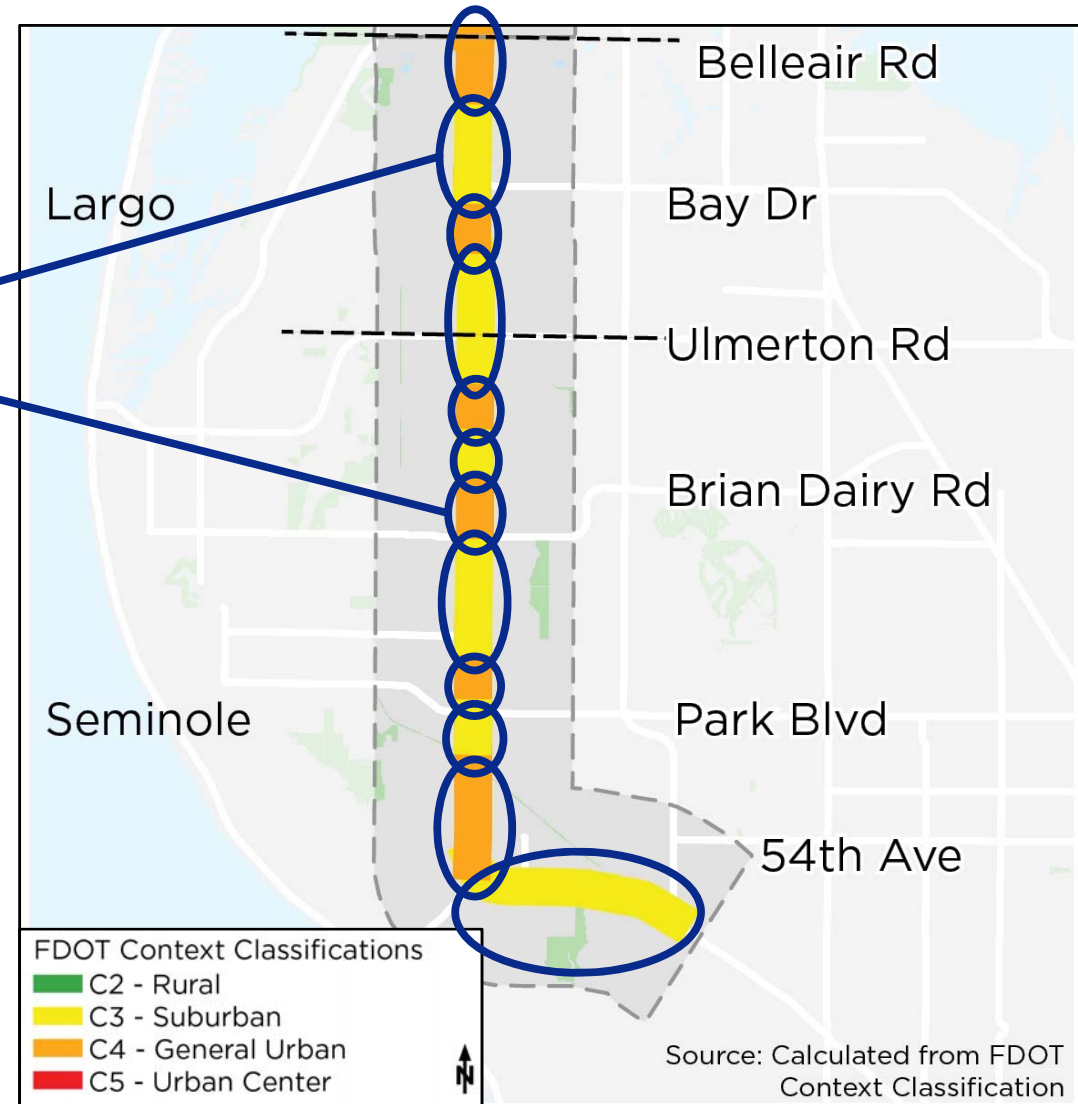




# Alternate US 19 South Context Classification

**C4** – General Urban

**C3** – Suburban





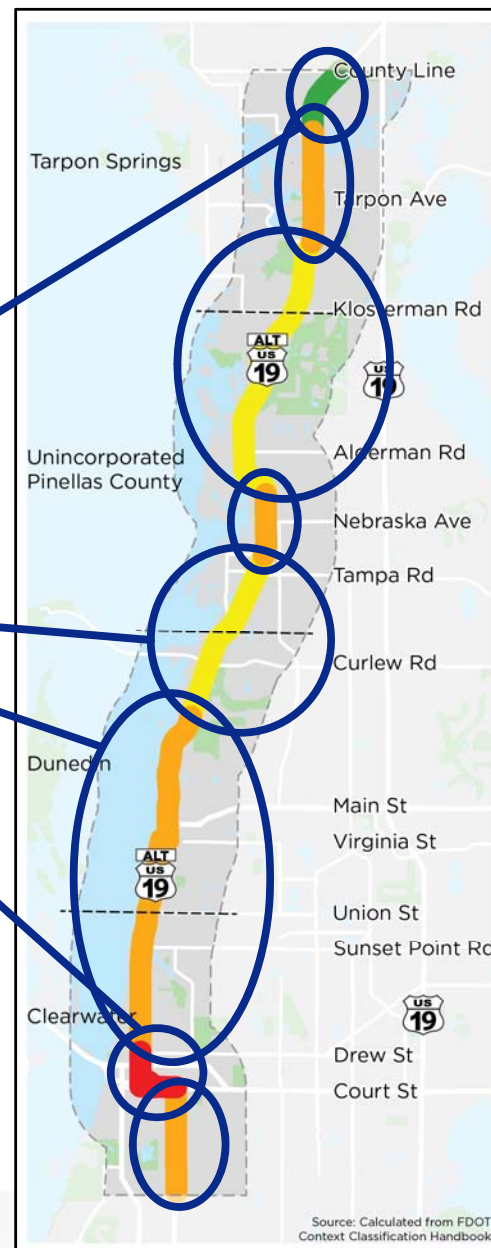
# Alternate US 19 North Context Classification

**C5** – Urban Center

**C4** – General Urban

**C3** – Suburban

**C2** – Rural



FDOT Context Classifications

- C2 - Rural
- C3 - Suburban
- C4 - General Urban
- C5 - Urban Center

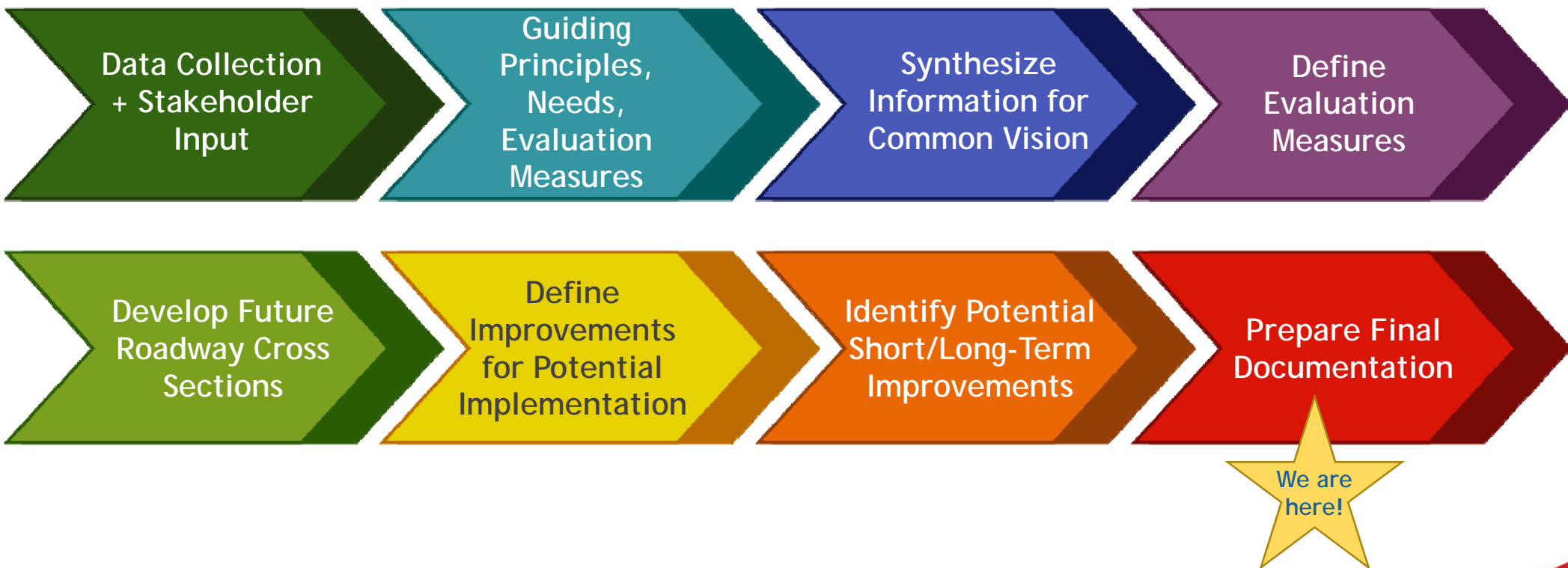
Alternate US 19 from Park Street N to Pinellas/Pasco County Line

Source: Calculated from FDOT  
Context Classification Handbook





# Study Process





# Alternatives Overview



Transportation Improvement  
Concept



# What kinds of improvements were considered for the Alternate US 19 Corridor?

- ▶ Turn lane improvements
- ▶ Better access management
- ▶ Roundabouts
- ▶ Bicycle lanes
- ▶ Pedestrian crosswalks

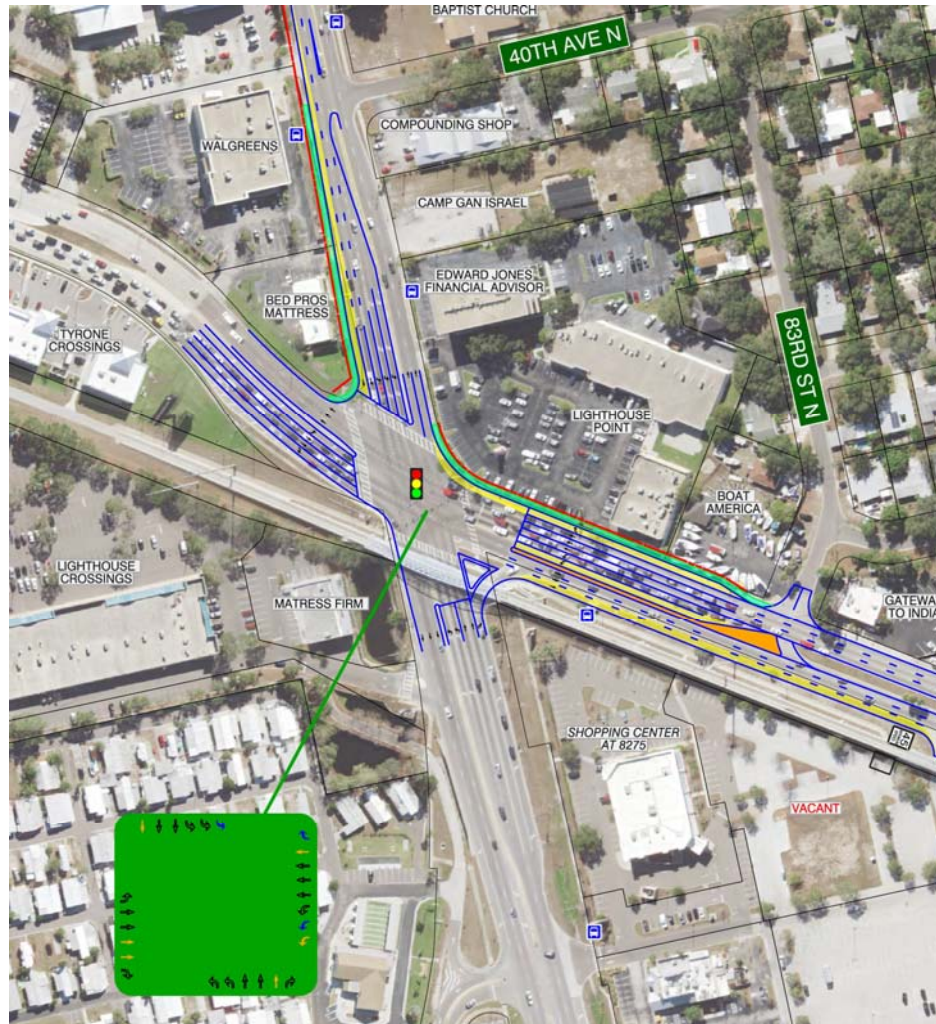






## Intersection Turn Lane Improvements

- ▶ Park Street North
- ▶ 100<sup>th</sup> Way
- ▶ Park Boulevard
- ▶ 102<sup>nd</sup> Avenue
- ▶ Lark Drive
- ▶ Ulmerton Road
- ▶ Bay Drive







## Intersection Turn Lane Improvements

- ▶ Park Street North
- ▶ 100<sup>th</sup> Way
- ▶ Park Boulevard
- ▶ 102<sup>nd</sup> Avenue
- ▶ Lark Drive
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## Intersection Turn Lane Improvements

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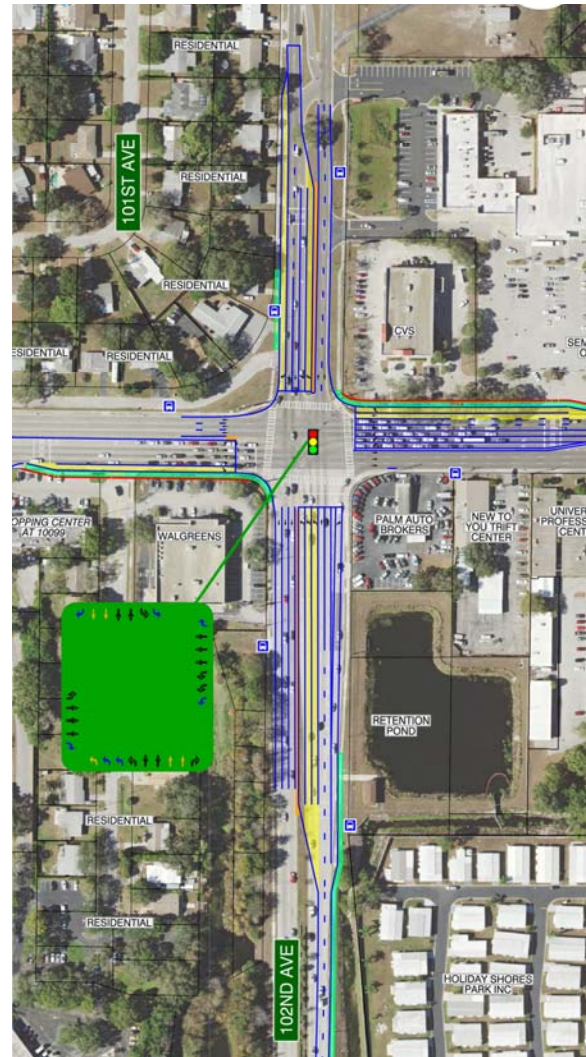






## Intersection Turn Lane Improvements

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## Intersection Turn Lane Improvements

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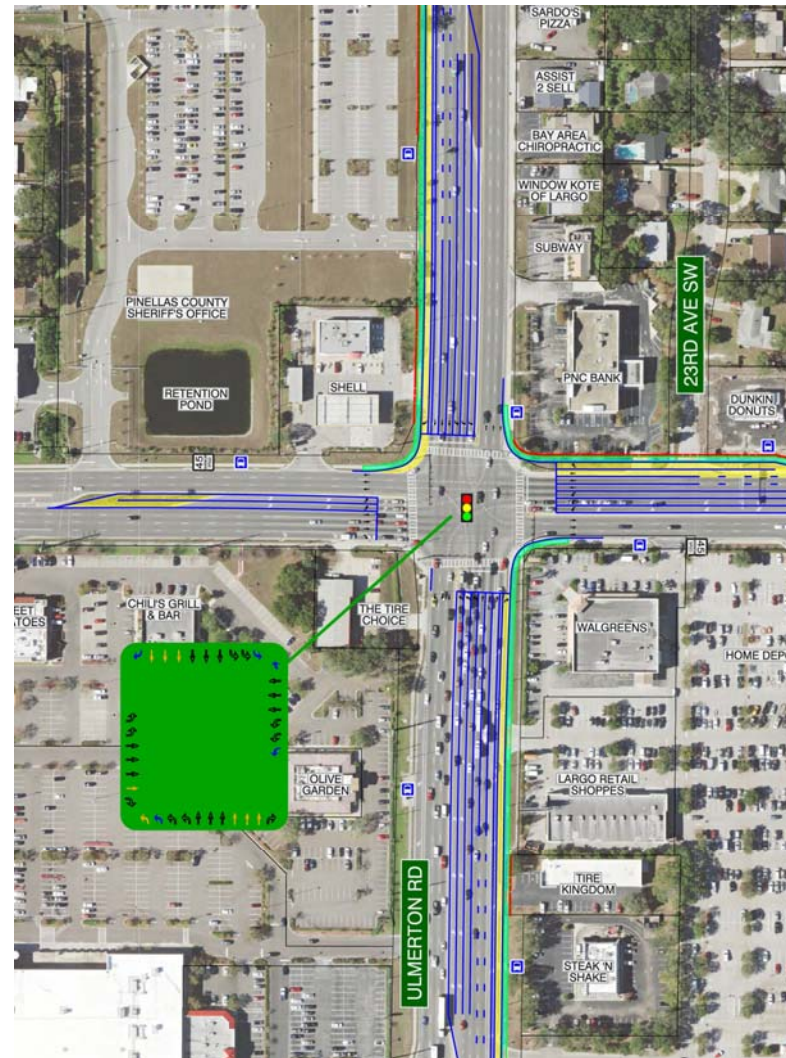






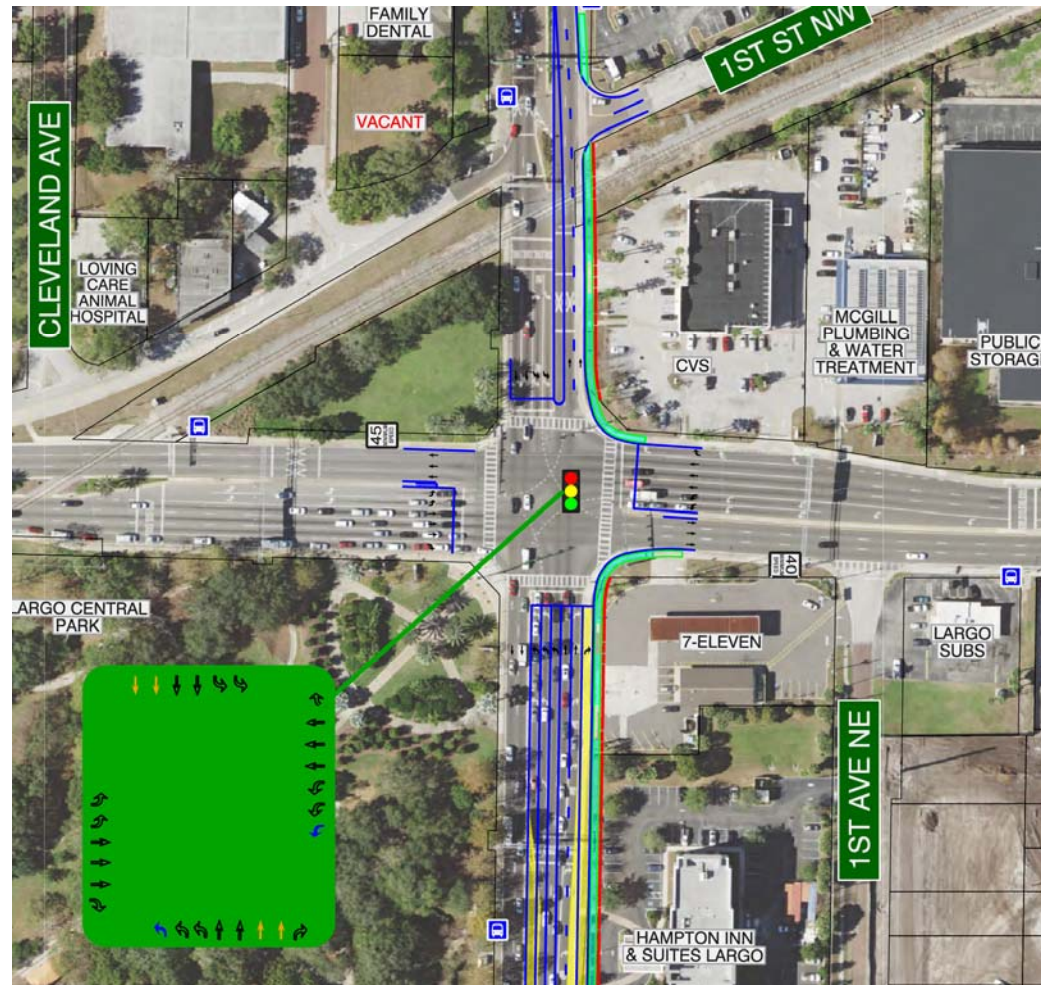
## Intersection Turn Lane Improvements

- ▶ Park Street North
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- ▶ 102<sup>nd</sup> Avenue
- ▶ Lark Drive
- ▶ **Ulmerton Road**
- ▶ Bay Drive



# Intersection Turn Lane Improvements

- ▶ Park Street North
- ▶ 100<sup>th</sup> Way
- ▶ Park Boulevard
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- ▶ Lark Drive
- ▶ Ulmerton Road
- ▶ Bay Drive







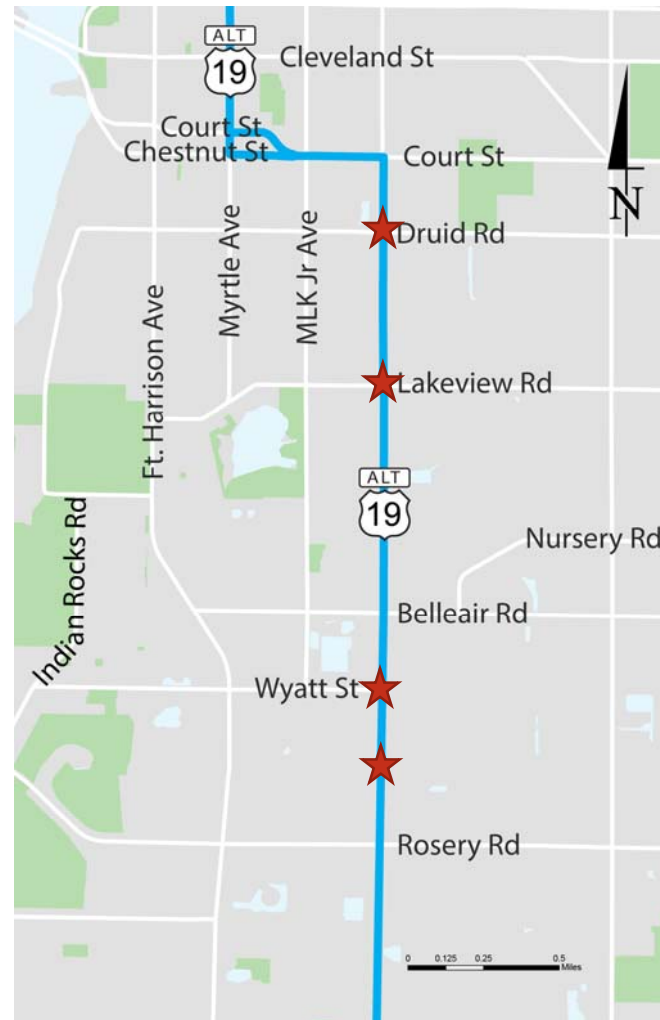
# Park Street North to 95<sup>th</sup> Street

- Widen the roadway and bridge from four (4) to six (6) lanes





# Largo and Clearwater - Repurpose Lanes



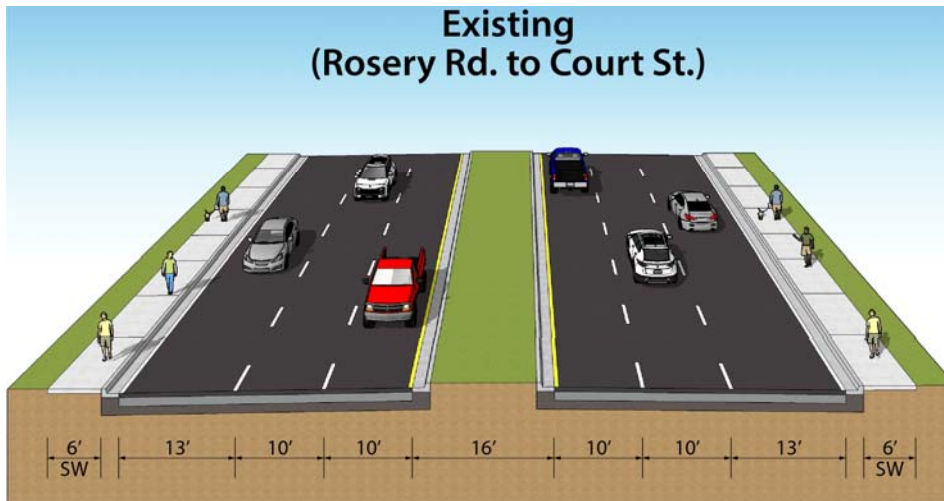
*Alternate US 19 from Park Street N to Pinellas/Pasco County Line*





# Largo and Clearwater - Repurpose Lanes

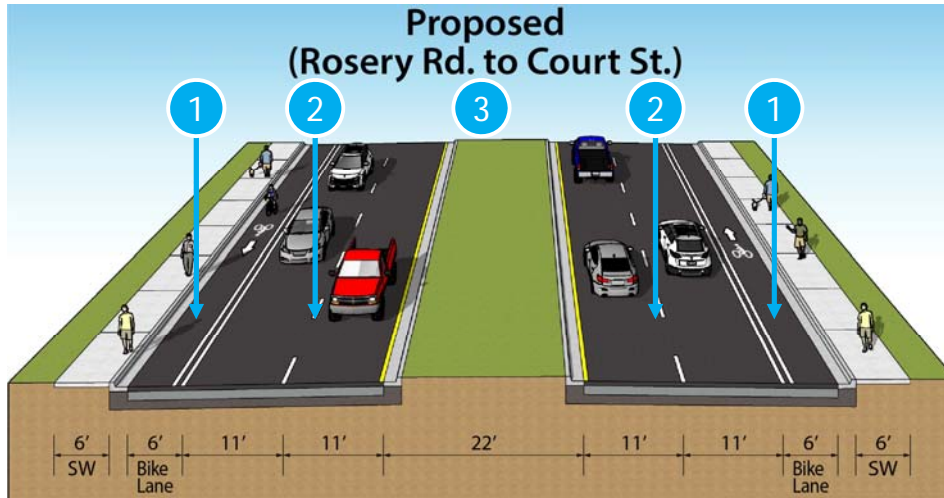
Existing  
(Rosery Rd. to Court St.)



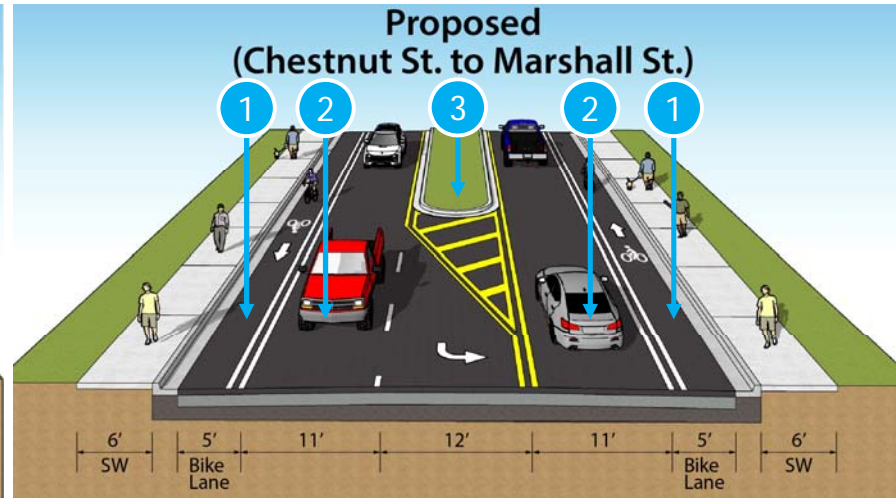
Existing  
(Chestnut St. to Marshall St.)



Proposed  
(Rosery Rd. to Court St.)



Proposed  
(Chestnut St. to Marshall St.)





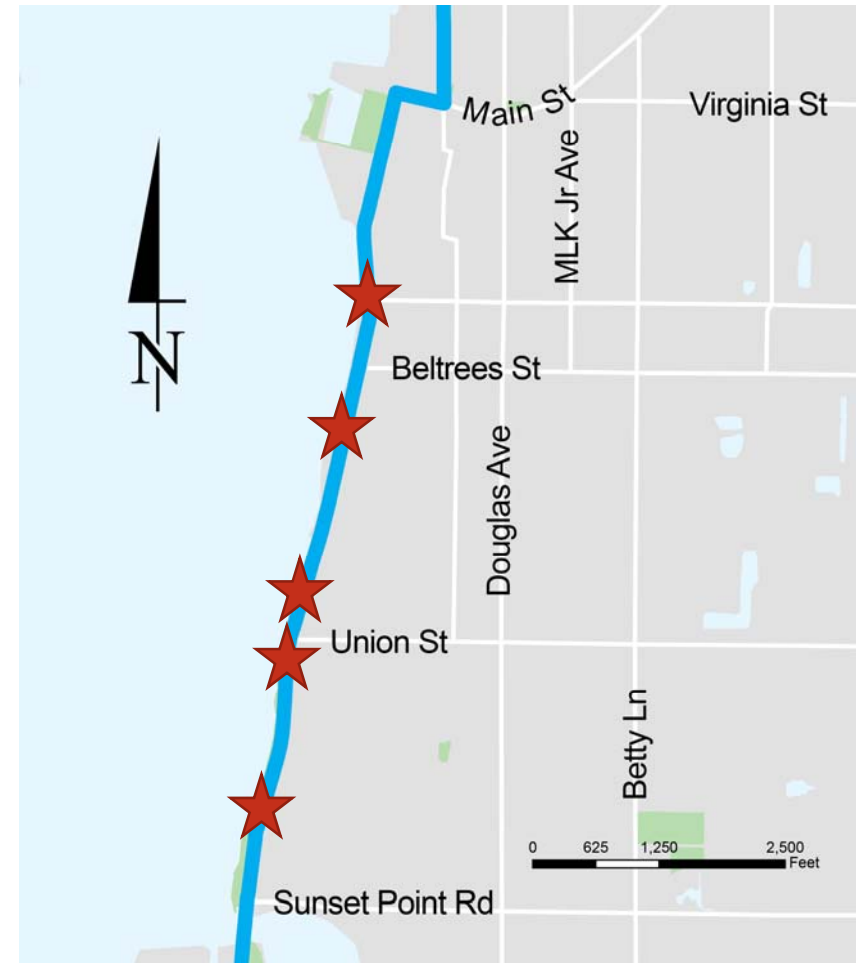
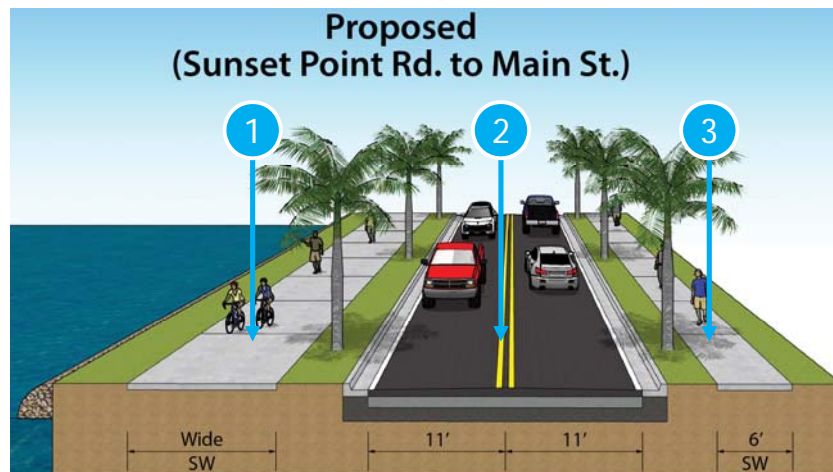


# Largo and Clearwater - Repurpose Lanes





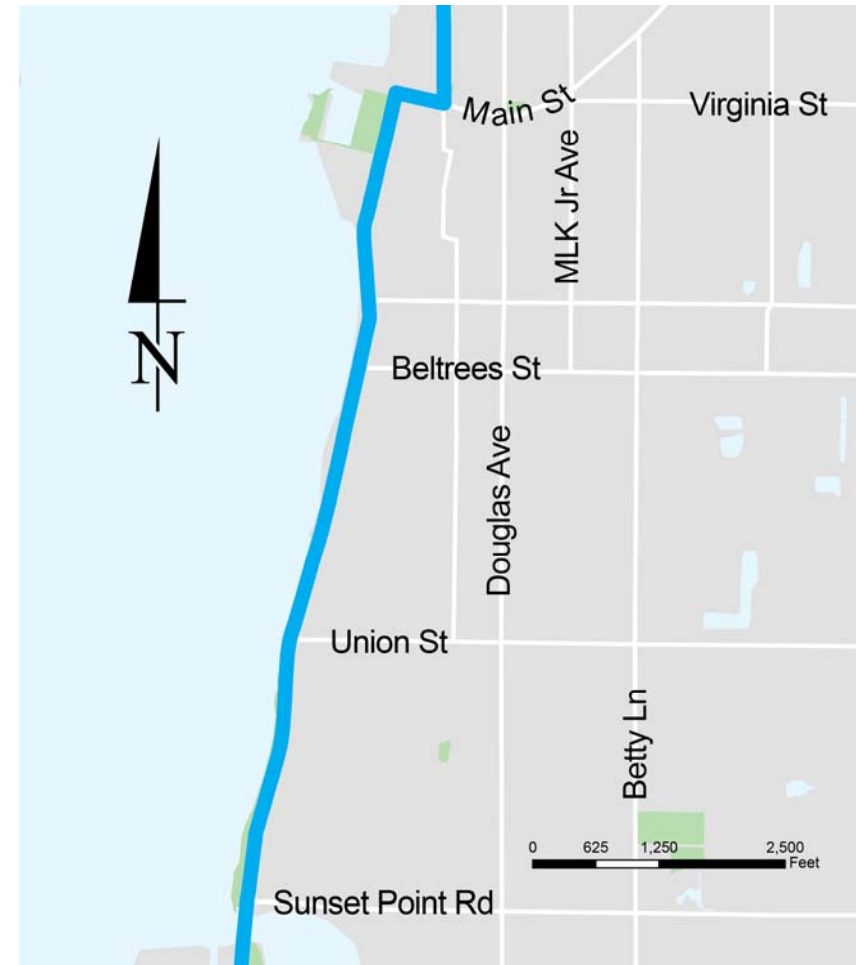
# Edgewater Drive Improvements







# Edgewater Drive Improvements



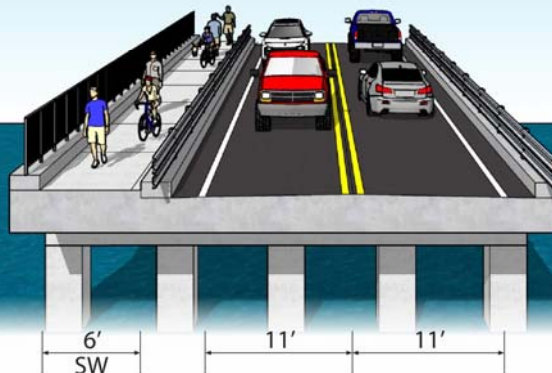
*Alternate US 19 from Park Street N to Pinellas/Pasco County Line*



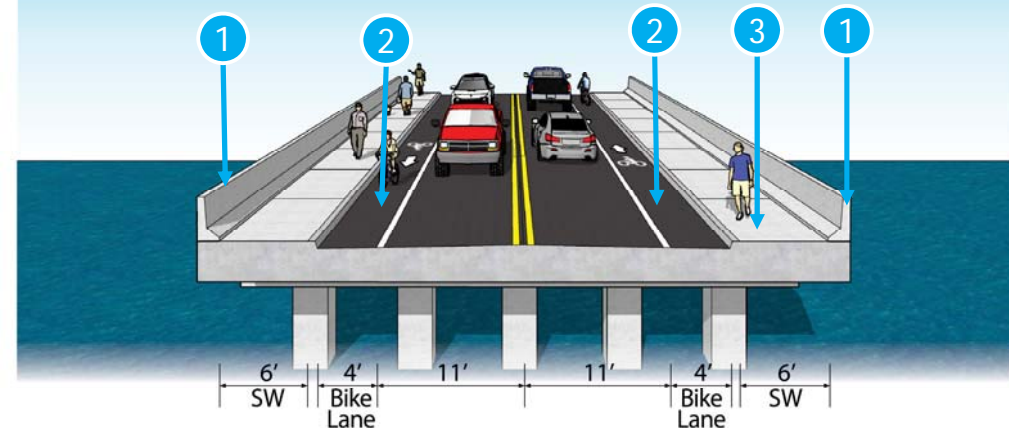


# Bridge Over Ancloste River

Existing  
(Bridge Over Ancloste River  
No. 150006)



Proposed





## Downtown Dunedin Evaluation Matrix

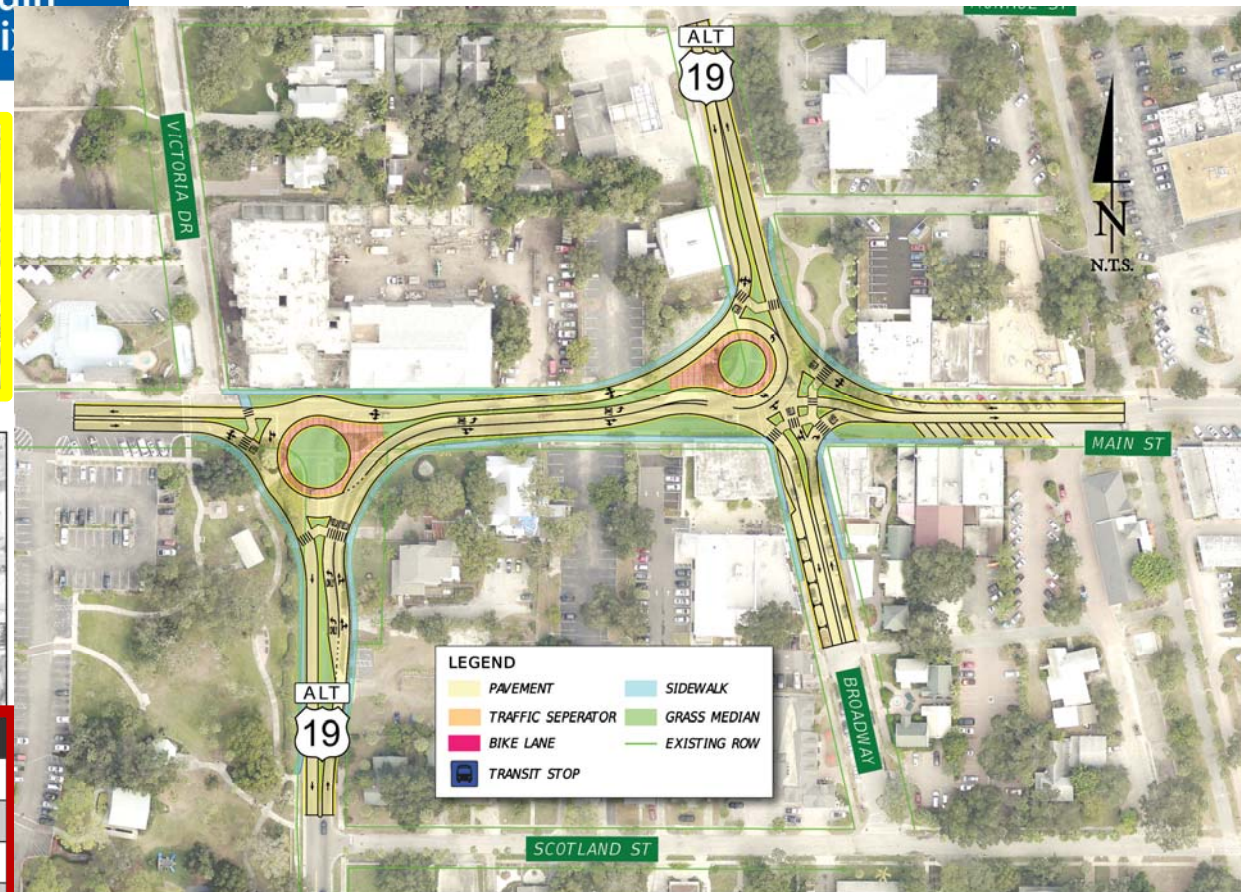
### Roundabout Alternative



### Signalized Alternative



Evaluation Measures	No Build	Roundabout	Signalized
Overall Intersection Delay (s)	117.6	38.2	24.8
Vehicular Conflict Points	41	12	41
Pedestrian/Bicycle Accommodations	Fair/Good	Good/Excellent	Good
Right-of-Way Impacts (Acres)	0.00	0.33	0.10
Construction Cost in \$ Million	\$0	\$2.2 M	\$1.5 M



## Alternate US 19 (SR 595) Corridor Study







## Downtown Dunedin Evaluation Matrix

### Roundabout Alternative



### Signalized Alternative



Evaluation Measures	No Build	Roundabout	Signalized
Overall Intersection Delay (s)	117.6	38.2	24.8
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Pedestrian/Bicycle Accommodations	Fair/Good	Good/Excellent	Good
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Construction Cost in \$ Million	\$0	\$2.2 M	\$1.5 M



## Alternate US 19 (SR 595) Corridor Study







# Curlew Rd/Causeway Blvd Evaluation Matrix

Add Lanes Alternative



Displaced Left Alternative



Continuous Flow Alternative

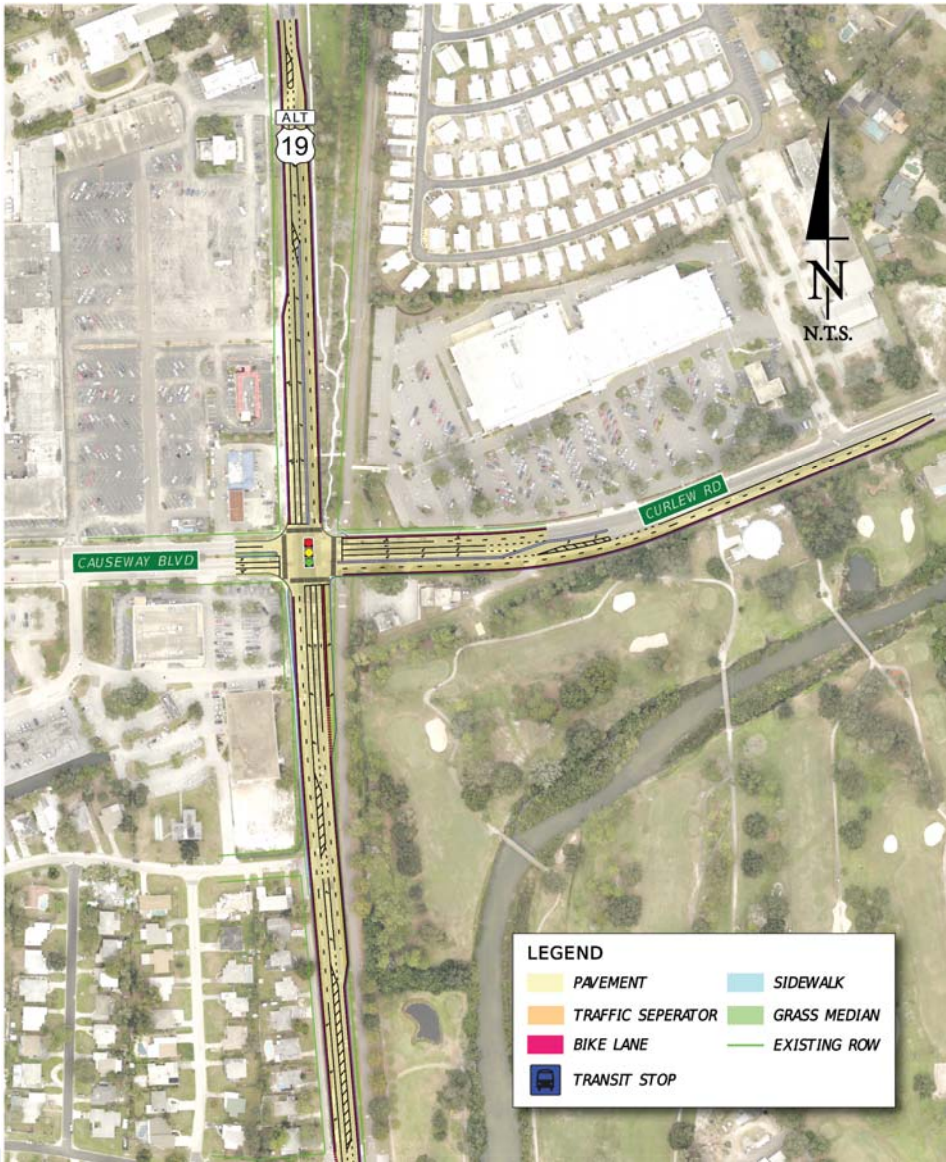


Median U-Turn Alternative



Evaluation Measures	No Build	Add Lanes	Displaced Left	Continuous Flow	Median U-Turn
Overall Intersection Delay (s)	93.0	54.8	45.8	41.8	55.0
Vehicular Conflict Points	32	32	30	28	16
Pedestrian/Bicycle Accommodations	Good	Good	Good	Good	Excellent
Right-of-Way Impacts (Acres)	0.00	0.02	0.03	0.11	0.84
Construction Cost in \$ Million	\$0	\$2.1 M	\$2.8 M	\$8.2 M	\$4.8 M

## Alternate US 19 (SR 595) Corridor Study







# Curlew Rd/Causeway Blvd Evaluation Matrix

Add Lanes Alternative



Displaced Left Alternative



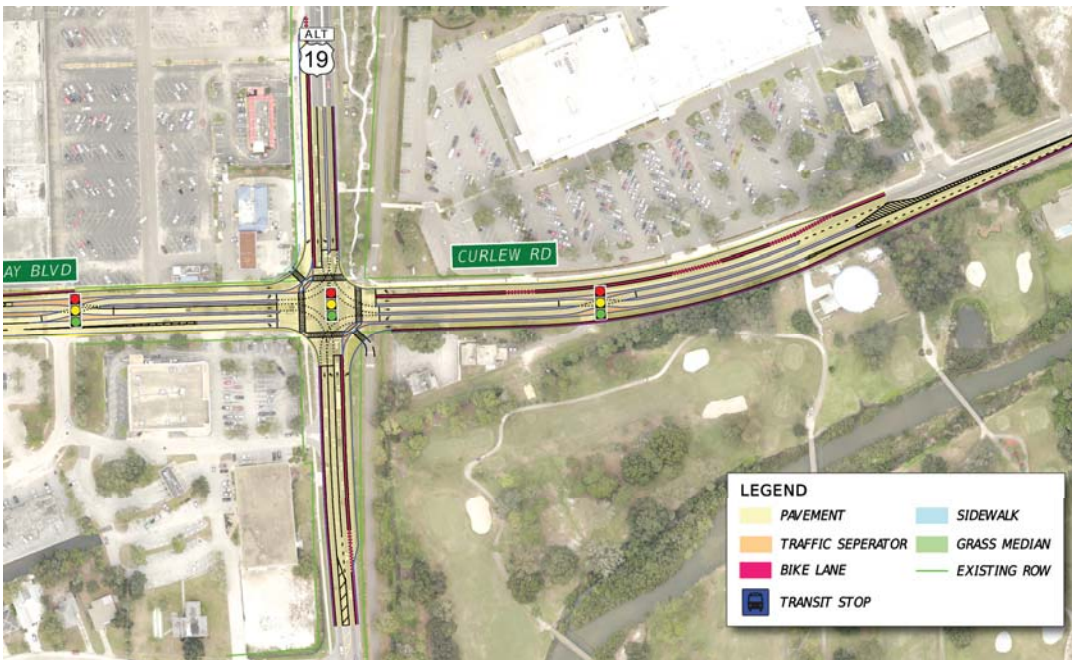
Continuous Flow Alternative



Median U-Turn Alternative



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## Alternate US 19 (SR 595) Corridor Study



Alternate US 19 from Park Street N to Pinellas/Pasco County Line





## Curlew Rd/Causeway Blvd Evaluation Matrix

**Add Lanes Alternative****Displaced Left Alternative****Continuous Flow Alternative****Median U-Turn Alternative**

Evaluation Measures	No Build	Add Lanes	Displaced Left	Continuous Flow	Median U-Turn
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## Alternate US 19 (SR 595) Corridor Study





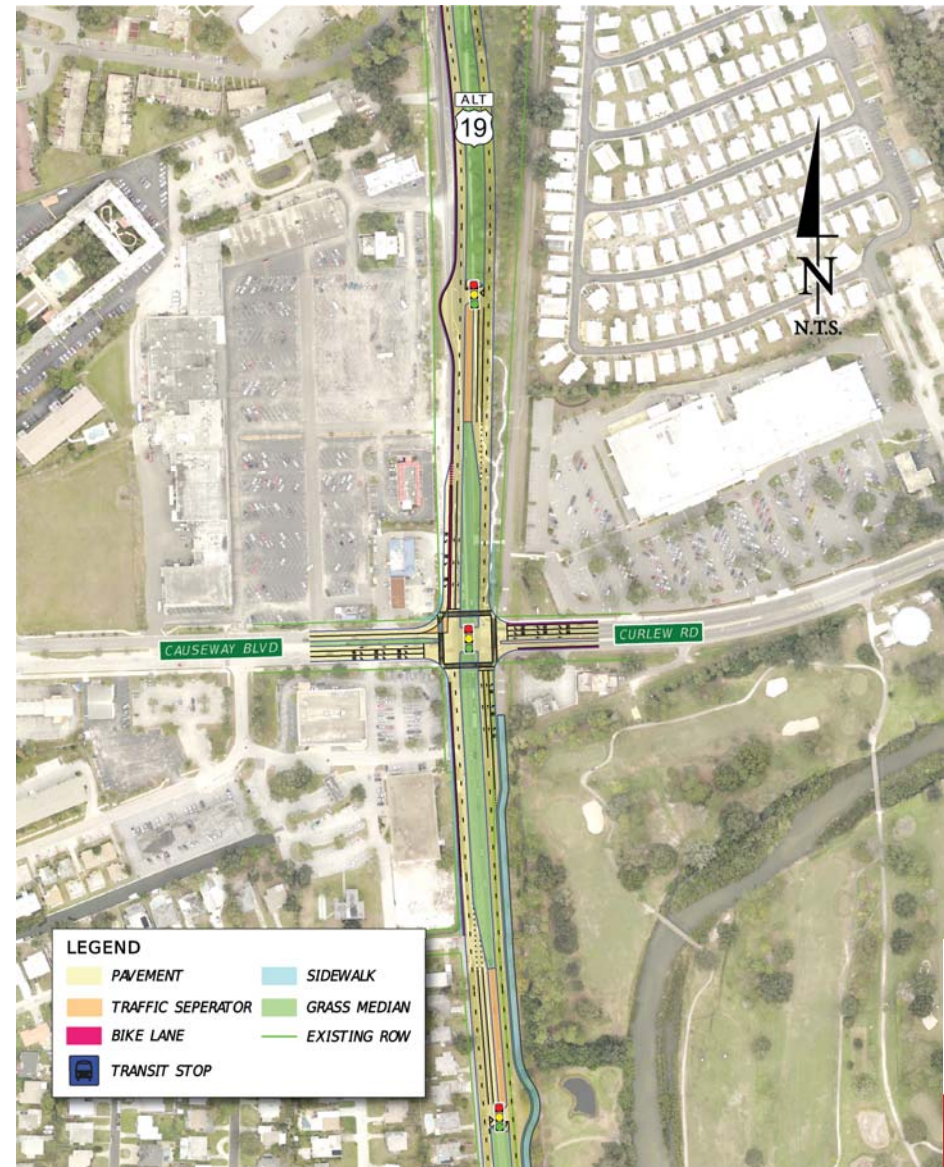


## Curlew Rd/Causeway Blvd Evaluation Matrix

**Add Lanes Alternative****Displaced Left Alternative****Continuous Flow Alternative****Median U-Turn Alternative**






























Evaluation Measures	No Build	Add Lanes	Displaced Left	Continuous Flow	Median U-Turn
Overall Intersection Delay (s)	93.0	54.8	45.8	41.8	55.0
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Construction Cost in \$ Million	\$0	\$2.1 M	\$2.8 M	\$8.2 M	\$4.8 M

Alternate US 19 (SR 595)  
Corridor Study





## Dunedin Evaluation Matrix

Guiding Principles		Objectives	No-Build Alternative	Build Alternative
	Match the Character	Roadway section aligns with context classification		
	Increase Safety	Improves intersection safety		
		Improves corridor safety for bicyclists and pedestrians		
	Balance User Needs	Maintains vehicular mobility		
		Maintains freight access		
		Provides pedestrian accommodations		
		Provides bicycle accommodations		
	Enhance the Networks	Provides access to local network		
		Provides bicycle/pedestrian crossings		
	Celebrate the Unique Assets	Protects environmental resources		
		Integrates historic and cultural elements		
		Provides streetscape opportunities		

- Meets Objective

- Somewhat Meets Objective

- Unmet Objective

## Alternate US 19 (SR 595) Corridor Study



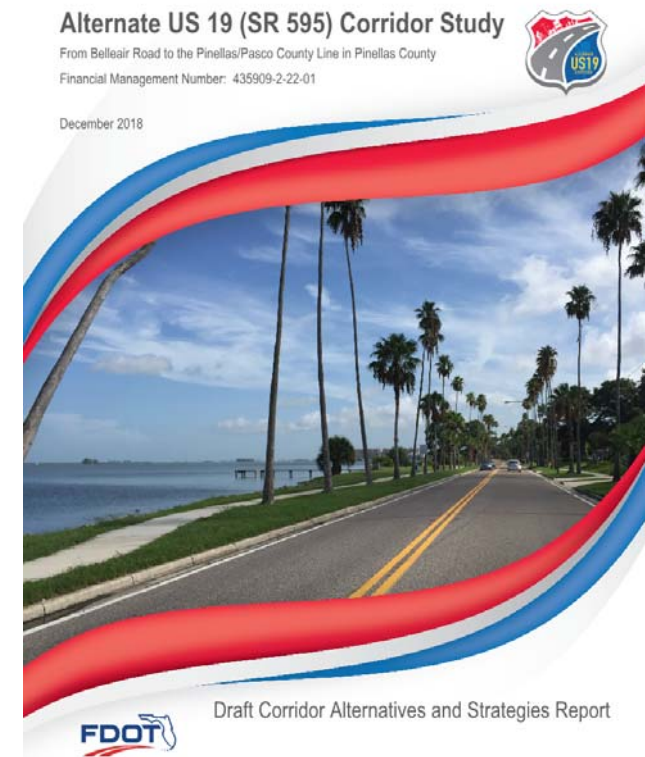




# Project Next Steps

- ▶ Finalize the improvement concepts and identify costs for short-term and long-term improvements
- ▶ Finalize documentation for the Alternate US 19 Corridor Studies
- ▶ Forward Pinellas/FDOT to prioritize improvements and identify funding

FDOT Work Program Phases	Date
Corridor Planning Study (PLAN)	2019
Project Development and Environment (PD&E)	TBD
Preliminary Engineering (PE)	2023
Right of Way (ROW)	TBD
Construction (CON)	Unfunded







# Project Contact Information

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Project Website: [www.fdotd7studies.com/altus19studies/](http://www.fdotd7studies.com/altus19studies/)



**Thank You for Your Participation!**